

CONTRACT	DESIGNATION
IR-33742	1006075

KIN DES. NUMBERS	
Designation No.	Description
1172100	I-69 NB OVER UNNAMED TRIBUTARY (UNT) OF CLEAR CREEK
1172101	I-69 SB OVER UNNAMED TRIBUTARY (UNT) OF CLEAR CREEK
1172102	I-69 OVER TRIBUTARY OF UNNAMED TRIBUTARY (UNT) OF CLEAR CREEK
1172104	I-69 NB OVER BOLIN LN.
1172105	I-69 SB OVER BOLIN LN.
1172111	I-69/S.R. 37 INTERCHANGE BRIDGE

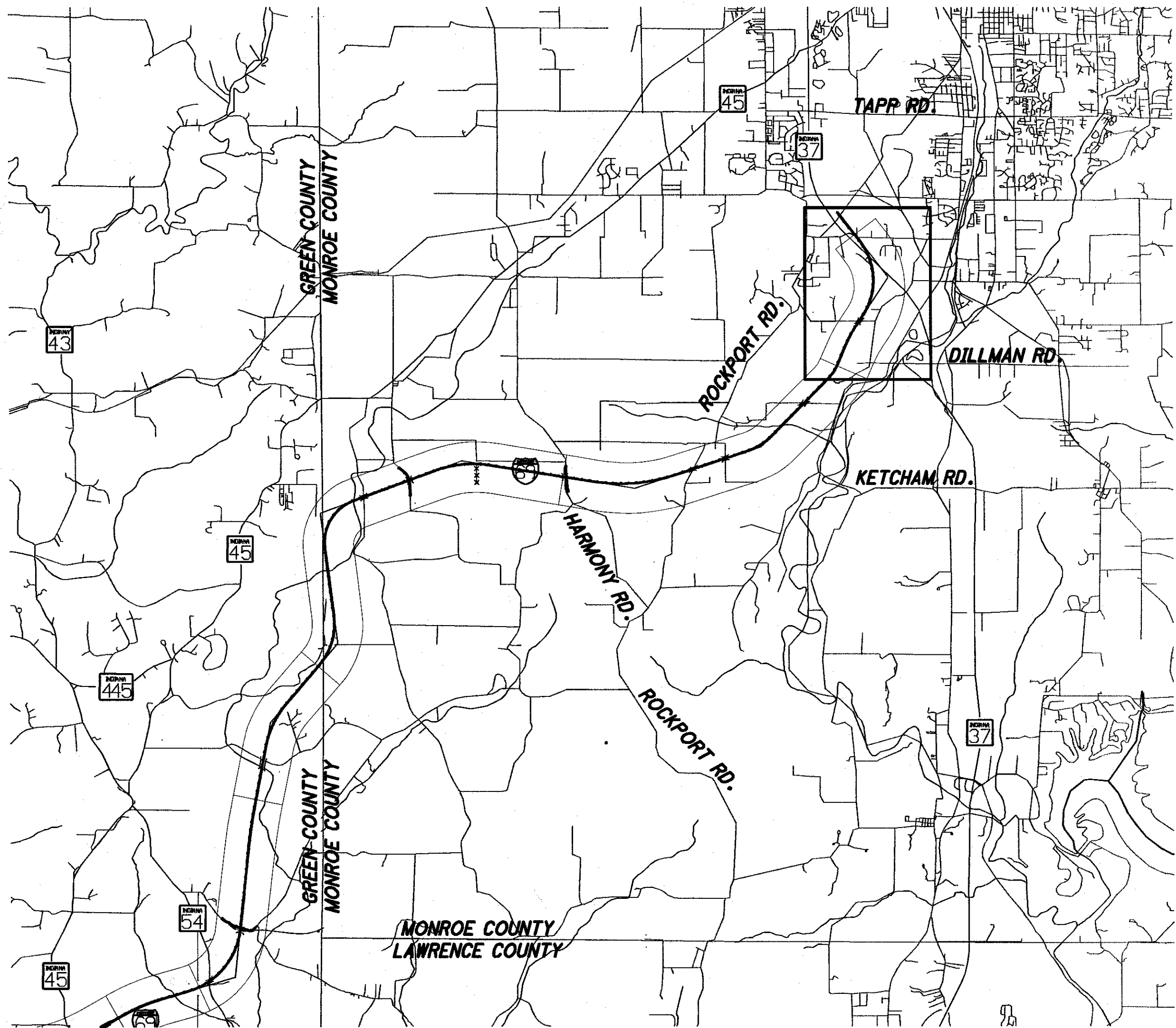
ADJACENT PROJECTS	
Designation No.	Description
0500450	I-69 - HARMONY ROAD TO BRANCH CLEAR CREEK (STA. 1255+00 TO 1463+97.42 LINE "A")

**INDIANA
DEPARTMENT OF
TRANSPORTATION**

ROAD PLANS

PROJECT NO. 1006075 PE, RW, CONST.
Route: I-69 From RP 112+83 To RP 114+63

This Project features New Freeway Construction on I-69 from Unnamed Tributary (UNT) of Clear Creek to S.R. 37. The design includes a new full service diamond interchange with four ramps at I-69 / SR 37. This project is located in Sections 19 and 30, Township 8 North, Range 1 West, Perry Township, Monroe County, Indiana.



Eq.
Sta. 1554+87.53 "PR-A" (BK) =
Sta. 216+76.01 "SR 37" (AH)

END PROJECT 1006075
STA. 1554+87.53 "PR-A"

New Bridge Construction
S.R. 37 Interchange (Line "PR-SR 37")
over I-69 N.B./S.B. (Line "PR-A")
Str. No. 169-53-9716, Des. No. 1172111

New Bridge Construction
I-69 over Bolin Lane
N.B. Str. No. 169-53-9710, Des. No. 1172104
S.B. Str. No. 169-53-9711, Des. No. 1172105

New Bridge Construction
I-69 over Tributary of Unnamed Tributary
(UNT) of Clear Creek
Str. No. 169-53-9709, Des. No. 1172102

New Bridge Construction
I-69 over Unnamed Tributary (UNT) of Clear Creek
N.B. Str. No. 169-53-9707, Des. No. 1172100
S.B. Str. No. 169-53-9708, Des. No. 1172101

BEGIN PROJECT 1006075
END PROJECT 0500450
STA. 1463+97.42 "A"



Scale: NTS
Location Map

[Signature] 11-26-12
I-69 REAL ESTATE MANAGER DATE

For Traffic & Design Data
See Index Sheet



PROJECT LOCATION SHOWN BY

Roadway Length (I-69):	1.685 mi
Total Length (I-69):	1.711 mi
Max. Grade :	3.240%
Begin Project Latitude:	39° 05' 35" N
Begin Project Longitude:	86° 33' 57" W
End Project Latitude:	39° 06' 55" N
End Project Longitude:	86° 33' 53" W
Hydrologic Unit Number:	05120208090020

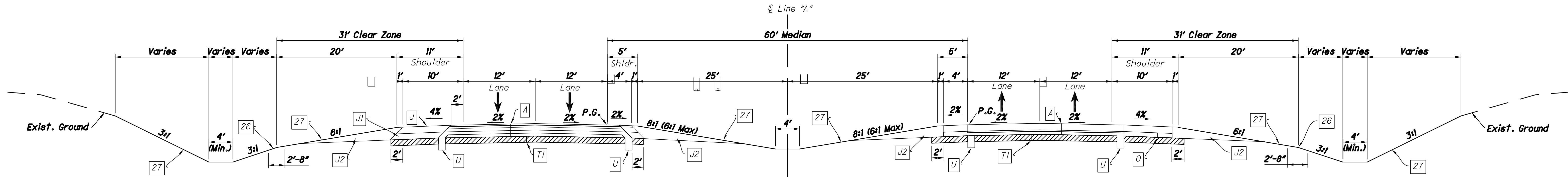
Stage 1 Submittal
November 23, 2011

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2012
TO BE USED WITH THESE PLANS

WilburSmith
ASSOCIATES

429 N. Pennsylvania St., Suite 409
Indianapolis, Indiana 46204
p 317.822.6269 f 317.822.6273
www.WilburSmith.com

FEDERAL HIGHWAY ADMINISTRATION U.S. DEPT. OF TRANSPORTATION APPROVED: _____ DATE _____ DIVISION ADMINISTRATOR		PLANS PREPARED BY: WILBUR-SMITH ASSOCIATES 317-829-6900 PHONE NUMBER CERTIFIED BY: _____ DATE _____ APPROVED FOR LETTING: _____ CHIEF, DIVISION OF DESIGN DATE _____		<table><tr><td colspan="2">BRIDGE FILE</td></tr><tr><td colspan="2">N/A</td></tr><tr><td colspan="2">DESIGNATION</td></tr><tr><td colspan="2">1006075</td></tr><tr><td>SURVEY BOOK</td><td>PAGE</td></tr><tr><td>ELECTRONIC / AERIAL</td><td>TS-01</td></tr><tr><td>CONTRACT</td><td>PROJECT</td></tr><tr><td>IR-33742</td><td>1006075</td></tr></table>		BRIDGE FILE		N/A		DESIGNATION		1006075		SURVEY BOOK	PAGE	ELECTRONIC / AERIAL	TS-01	CONTRACT	PROJECT	IR-33742	1006075
BRIDGE FILE																					
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1006075																					
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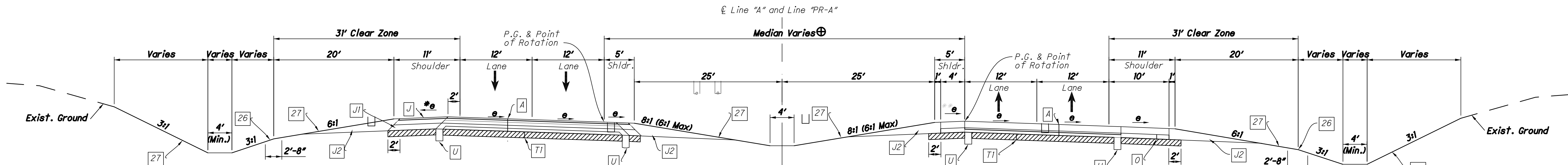


Southbound Lanes
HMA Pavement Option

Typical Section I-69

Scale: 1/8" = 1'-0"
Sta. 1463+97.42 to Sta. 1464+31.05 (NB) Line "A"
Sta. 1464+68.39 to Sta. 1469+34.00 (NB) Line "A"
Sta. 1463+97.42 to Sta. 1464+63.61 (SB) Line "A"
Sta. 1468+00.95 to Sta. 1469+34.00 (SB) Line "A"
Sta. 1488+32.00 to Sta. 1492+05.00 Line "A"

Northbound Lanes
PCCP Pavement Option



Southbound Lanes

Typical Superelevated Section I-69

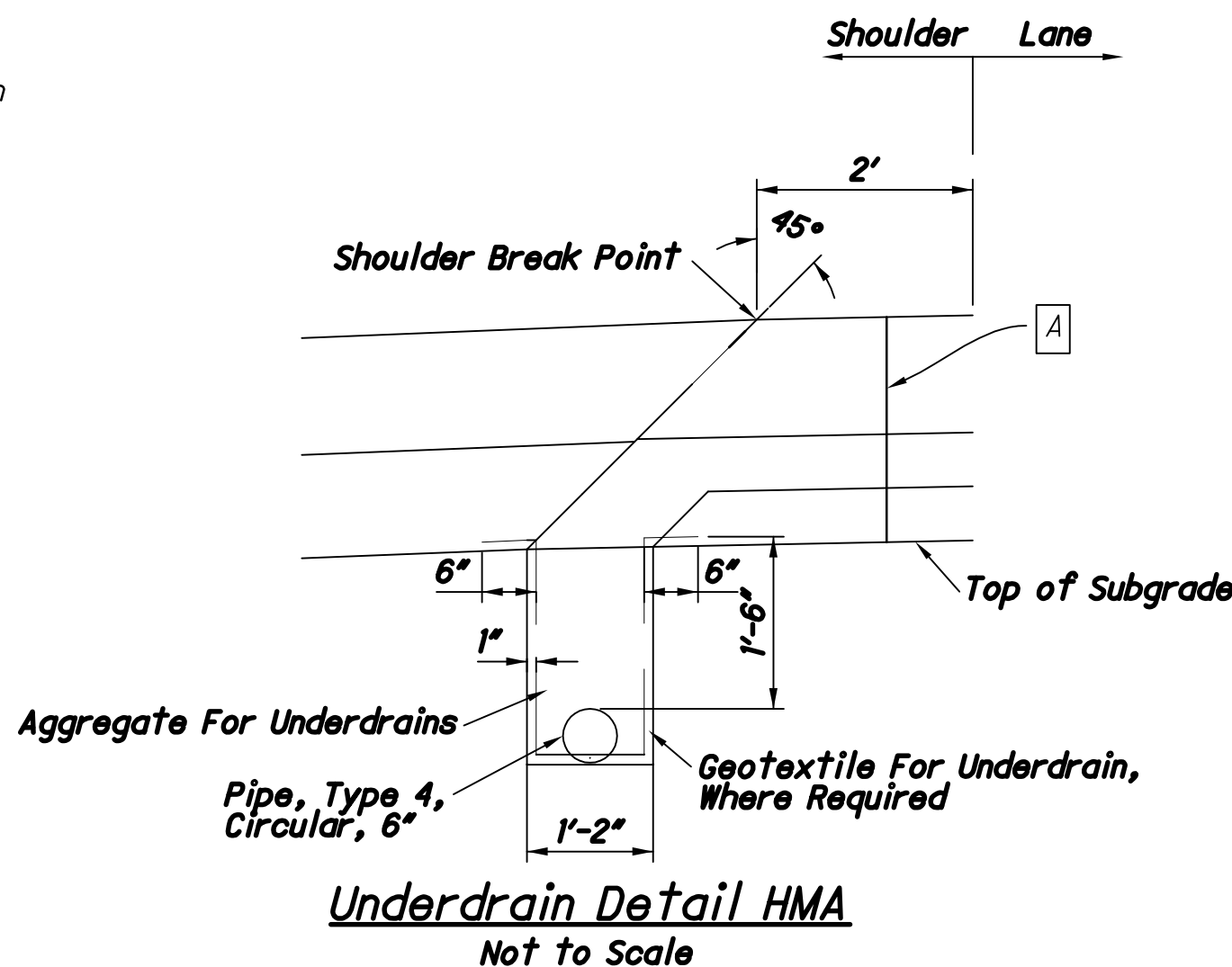
Scale: 1/8" = 1'-0"
Reverse As Necessary
Sta. 1469+34.00 to Sta. 1488+32.00 Line "A" emax = 2.7% (RT)
Sta. 1492+05.00 Line "A" to Sta. 1493+34.75 Line "A" (NB) emax = 5.2% (LT)
Sta. 1494+70.60 "PR-A" to Sta. 1525+25.00 Line "PR-A" (NB) emax = 5.2% (LT)
Sta. 1492+05.00 Line "A" to Sta. 1492+76.62 Line "A" (SB) emax = 5.2% (LT)
Sta. 1494+13.10 "PR-A" to Sta. 1525+25.00 Line "PR-A" (SB) emax = 5.2% (LT)

Northbound Lanes
PCCP Pavement Option

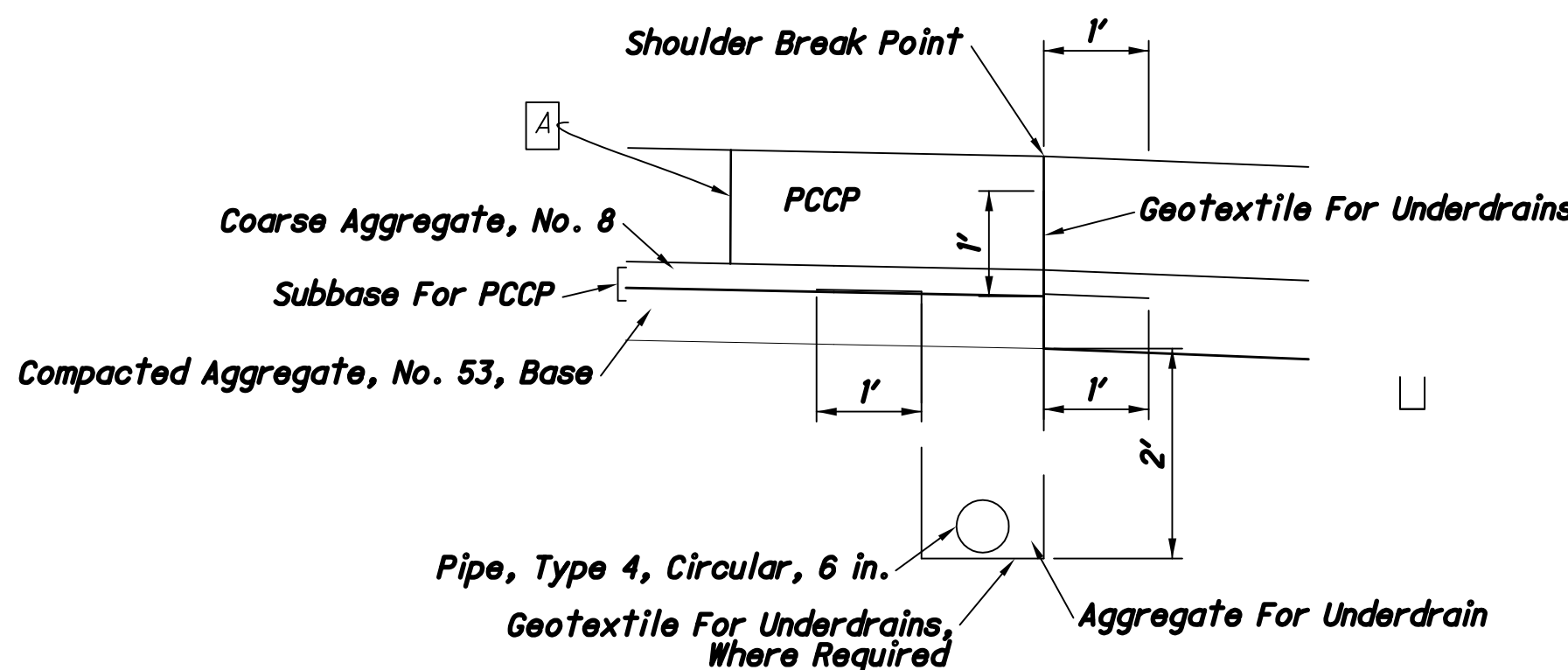
** For High Side Shoulder Width Greater Than 4', Cross Slope is 4% if e is Less Than or Equal To 4%, if e is Greater Than 4% But Less Than Or Equal To 6% Then The Cross Slope Is 2%.

⊕ Median width = 60'-0" from 1469+34.00 Line "A" to Sta. 1513+00.00 Line "PR-A"
Median Varies from 60'-0" at Sta. 1513+00.00 to 35'-0" at Sta. 1523+50.00 Line "PR-A"
Median Varies from 35'-0" at Sta. 1523+50.00 to 30'-6" at Sta. 1525+25.00 Line "PR-A"
Median width = 30'-6" from 1525+25.00 Line "PR-A" to Sta. 1549+63.00 Line "PR-A"

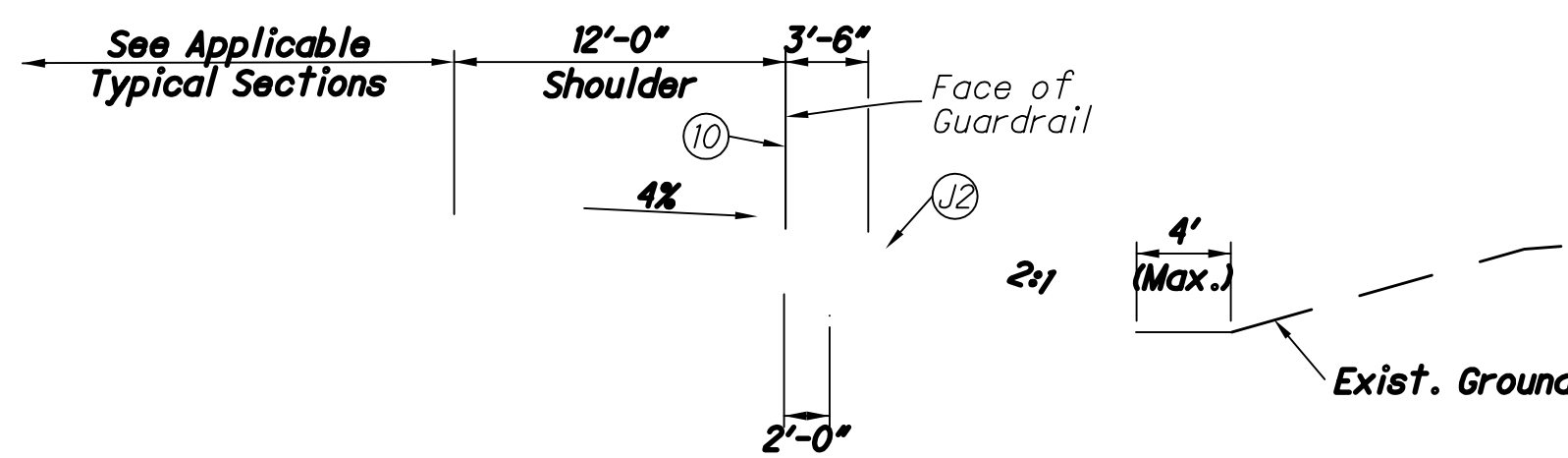
- (A) PCCP, 11" on Subbase For PCCP or 165 lbs/Sys QC/QA-HMA, 4, 76, Surface, 9.5 mm on 275 lbs/Sys QC/QA-HMA, 4, 76, Intermediate, 19.0 mm on 330 lbs/Sys QC/QA-HMA, 4, 64, Base, 19.0 mm on 250 lbs/Sys QC/QA-HMA, 5, 76, Intermediate, OG 19.0 mm on 385 lbs/Sys QC/QA-HMA, 4, 64, Base, 19.0 mm
- (J) 165 lbs/Sys QC/QA-HMA, 1, 64, Surface, 9.5 mm on 330 lbs/Sys QC/QA-HMA, 1, 64, Intermediate, 19.0 mm
- (J1) 880 lbs/Sys QC/QA-HMA, 1, 64, Base, 25.0 mm
- (J2) 8" Compacted Aggregate, No. 53
- (O) 8" Compacted Aggregate, No. 53, Base
- (T1) Subgrade Treatment, Type IA
- (T2) Subgrade Treatment, Type IIA
- (T3) Subgrade Treatment, Type IIIA
- (U) 6" Underdrain
- (10) Guardrail
- (26) Sodding
- (27) Seeding, R



Underdrain Detail HMA
Not to Scale



Underdrain Detail Shoulder PCCP
Not to Scale



Shoulder w/Guardrail
Not to Scale

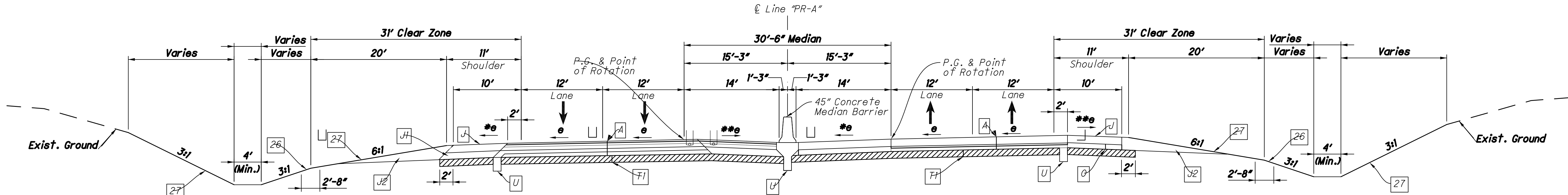
Note: All Shoulders On I-69 Shall Have Milled Shoulder Corrugations, Per INDOT STD. DWG. E606-SHCG-01 E606-SHCG-02 E606-SHCG-03

RECOMMENDED FOR APPROVAL	
DESIGNED: MDO	DRAWN: KCH
CHECKED: HCF	CHECKED: MDO

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
I-69 - LINE "A" & LINE "PR-A"

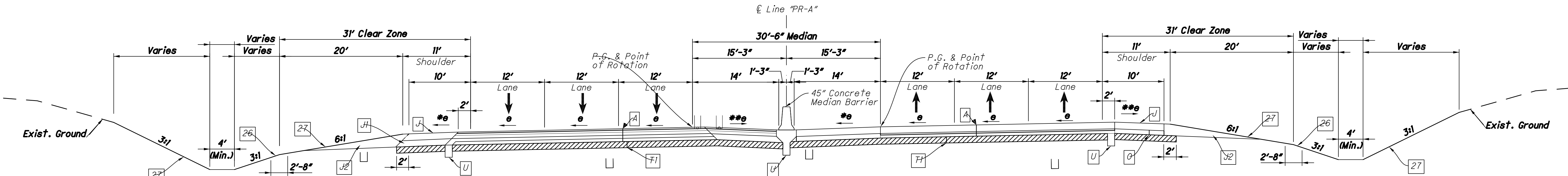
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AS NOTED	N/A
VERTICAL SCALE	DESIGNATION
AS NOTED	1006075
SURVEY BOOK	PAGE
ELECTRONIC / AERIAL	TY-01
CONTRACT	PROJECT
IR-33742	1006075



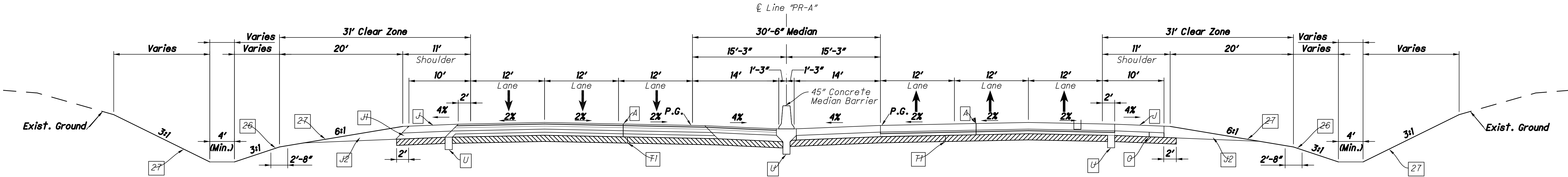
*For Low Side Shoulder Width Greater Than 4', Cross Slope is 4% if e is Less Than or Equal To 4%, if e is Greater Than 4% Then The Cross Slope Matches Adjacent Travel Lane

Southbound Lanes
HMA Pavement Option
Typical Urban Superelevated 2-lane Section I-69
Scale: 1/4" = 1'-0"
Reverse As Necessary
Sta. 1525+25.00 to Sta. 1540+60.00 Line "PR-A" (RT) e_{max} = 5.2% (RT)
Sta. 1525+25.00 to Sta. 1546+90.00 Line "PR-A" (LT) e_{max} = 5.2% (RT)

**For High Side Shoulder Width Greater Than 4', Cross Slope is 4% if e is Less Than or Equal To 4%, if e is Greater Than 4% But Less Than Or Equal To 6% Then The Cross Slope Is 2%.



Southbound Lanes
HMA Pavement Option
Typical Urban Superelevated 3-Lane Section I-69
Scale: 1/4" = 1'-0"
Reverse As Necessary
Sta. 1540+60.00 to Sta. 1549+63.00 Line "PR-A" (RT) e_{max} = 5.2% (RT)
Sta. 1546+90.00 to Sta. 1549+63.00 Line "PR-A" (LT) e_{max} = 5.2% (RT)



- (A) PCCP, 11" on Subbase For PCCP or 165 lbs/Sys QC/QA-HMA, 4, 76, Surface, 9.5 mm on 275 lbs/Sys QC/QA-HMA, 4, 76, Intermediate, 19.0 mm on 330 lbs/Sys QC/QA-HMA, 4, 64, Base, 19.0 mm on 250 lbs/Sys QC/QA-HMA, 5, 76, Intermediate, OG 19.0 mm on 385 lbs/Sys QC/QA-HMA, 4, 64, Base, 19.0 mm
- (J) 165 lbs/Sys QC/QA-HMA, 1, 64, Surface, 9.5 mm on 330 lbs/Sys QC/QA-HMA, 1, 64, Intermediate, 19.0 mm
- (JI) 880 lbs/Sys QC/QA-HMA, 1, 64, Base, 25.0 mm
- (J2) 8" Compacted Aggregate, No. 53
- (O) 8" Compacted Aggregate, No. 53, Base
- (T1) Subgrade Treatment, Type IA
- (T2) Subgrade Treatment, Type IIA
- (T3) Subgrade Treatment, Type IIIA
- (U) 6" Underdrain
- (10) Guardrail
- (26) Sodding
- (27) Seeding, R

Southbound Lanes
HMA Pavement Option
Typical Urban 3-Lane Section I-69
Scale: 1/4" = 1'-0"
Sta. 1549+63.00 to Sta. 1554+30.00 Line "PR-A" (RT)
Sta. 1549+63.00 to Sta. 1554+30.00 Line "PR-A" (LT)

Note: All Shoulders On I-69 Shall Have Milled Shoulder Corrugations, Per INDOT STD. DWG. E606-SHCG-01 E606-SHCG-02 E606-SHCG-03

		RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER _____ DATE _____		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE AS NOTED		BRIDGE FILE N/A	
		DESIGNED: _____ MDO _____ CHECKED: _____ HCF _____		DRAWN: _____ KCH _____ CHECKED: _____ MDO _____		VERTICAL SCALE AS NOTED		DESIGNATION 1006075	
				TYPICAL SECTIONS I-69 - LINE "PR-A"		SURVEY BOOK ELECTRONIC / AERIAL		PAGE TY-02	
						CONTRACT IR-33742		SHEETS 4 of 53	
						PROJECT 1006075			

CLEAR ZONE

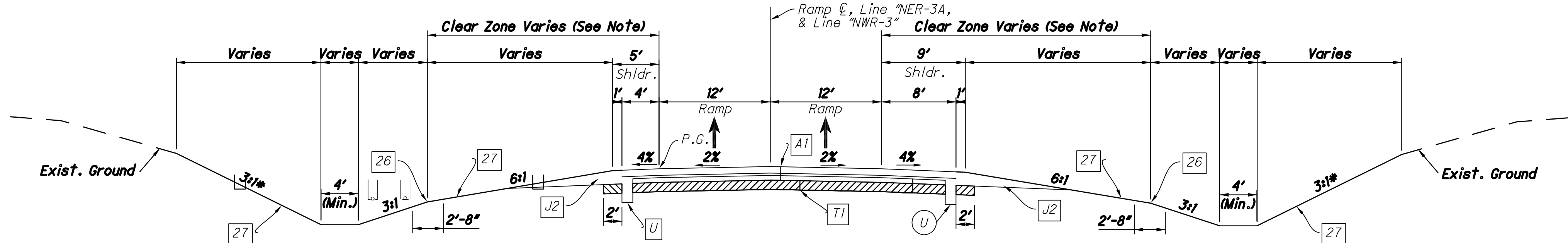
Line "NER-3A" Clear Zone:
From 1+00.00 to 9+00.00 "NER-3A", Clear Zone = 18'-0"
From 9+00.00 to 21+00.00 "NER-3A", Clear Zone = 14'-0"
From 21+00.00 to 26+60.12 "NER-3A", Clear Zone = 18'-0"

Line "NER-3B" Clear Zone:
From 1+00.00 to 3+87.01 "NER-3B", Clear Zone = 7'-0"

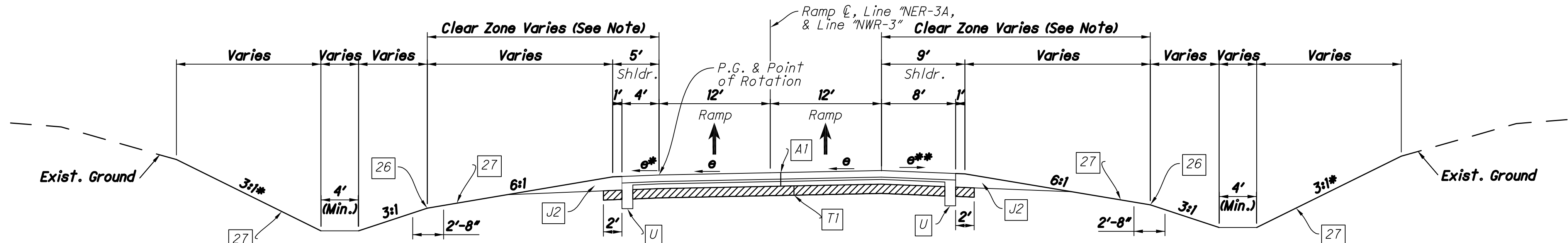
Line "NWR-3" Clear Zone:
From 1+00.00 to 16+50.00 "NWR-3", Clear Zone = 18'-0"
From 16+50.00 to 19+88.86 "NWR-3", Clear Zone = 14'-0"

Line "SER-3" Clear Zone:
From 1+00.00 to 10+00.00 "SER-3", Clear Zone = 12'-0"
From 10+00.00 to 19+00.00 "SER-3", Clear Zone = 7'-0"

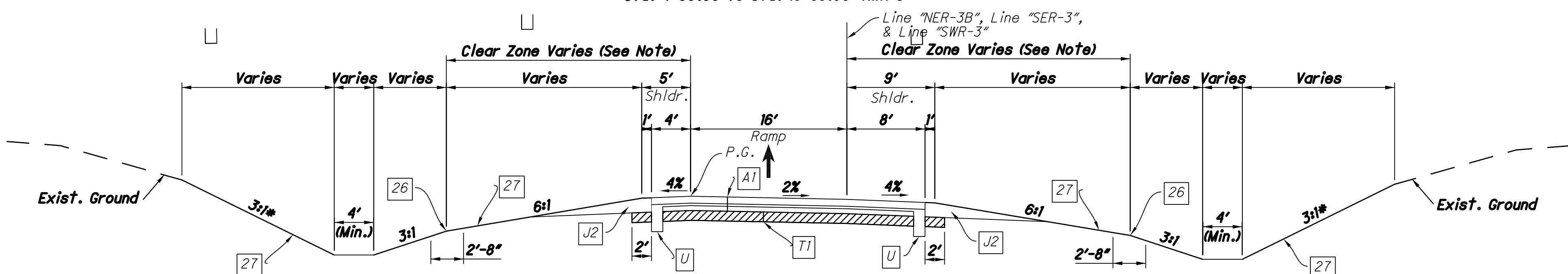
Line "SWR-3" Clear Zone:
From 1+00.00 to 7+00.00 "SWR-3", Clear Zone = 7'-0"
From 7+00.00 to 21+42.85 "SWR-3", Clear Zone = 12'-0"



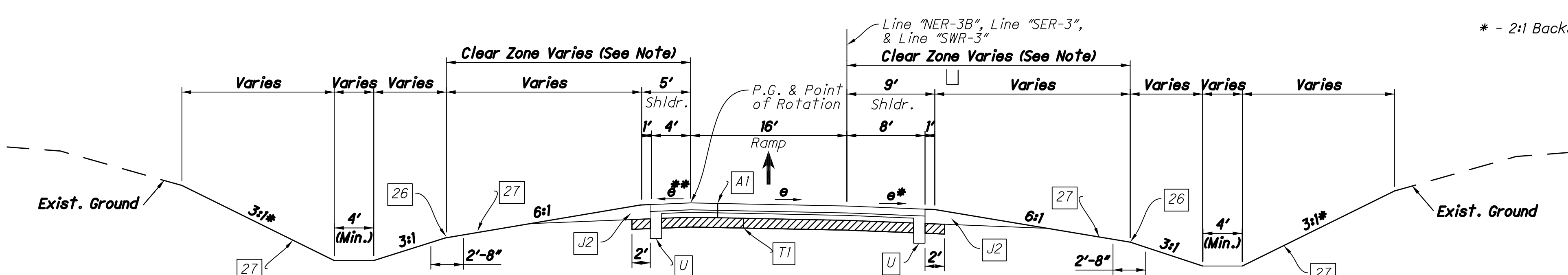
**Typical Two Lane Ramp ("NER-3A" and "NWR-3")
Tangent Section**
Scale: 1/4" = 1'-0"
Sta. 8+69.00 to Sta. 10+78.00 "NER-3A"
Sta. 20+25.00 to Sta. 22+53.82 "NER-3A"
Sta. 16+58.00 to Sta. 18+37.22 "NWR-3"



**Typical Two Lane Ramp ("NER-3A" and "NWR-3")
Superelevated Section (Reverse as necessary)**
Scale: 1/4" = 1'-0"
Sta. 1+00.00 to Sta. 8+69.00 "NER-3A"
Sta. 10+78.00 to Sta. 20+25.00 "NER-3A"
Sta. 4+90.00 to Sta. 16+58.00 "NWR-3"



**Typical One Lane Ramp ("SER-3" and "SWR-3")
Tangent Section**
Scale: 1/4" = 1'-0"
Sta. 7+45.00 to Sta. 10+85.00 "SER-3"
Sta. 14+46.00 to Sta. 17+59.90 "SER-3"
Sta. 1+83.00 to Sta. 2+95.00 "SWR-3"
Sta. 7+24.00 to Sta. 7+74.00 "SWR-3"



**Typical One Lane Ramp ("NER-3B", "SER-3", & "SWR-3")
Superelevated Section (Reverse as necessary)**
Scale: 1/4" = 1'-0"
Sta. 1+66.00 to Sta. 3+85.00 "NER-3B"
Sta. 5+08.32 to Sta. 7+45.00 "SER-3"
Sta. 10+85.00 to Sta. 14+46.00 "SER-3"
Sta. 2+95.00 to Sta. 7+24.00 "SWR-3"
Sta. 7+74.00 to Sta. 17+81.00 "SWR-3"

****For High Side Shoulder Width Greater Than 4', Cross Slope is 4% If e is Less Than or Equal to 4%, If e is Greater Than 4% But Less Than Or Equal To 6% Then The Cross Slope Is 2%.**

***For Low Side Shoulder Width Greater Than 4', Cross Slope is 4% If e is Less Than or Equal to 4%, If e is Greater Than 4% Then The Cross Slope Matches Adjacent Travel Lane**

* - 2:1 Backslope Max.

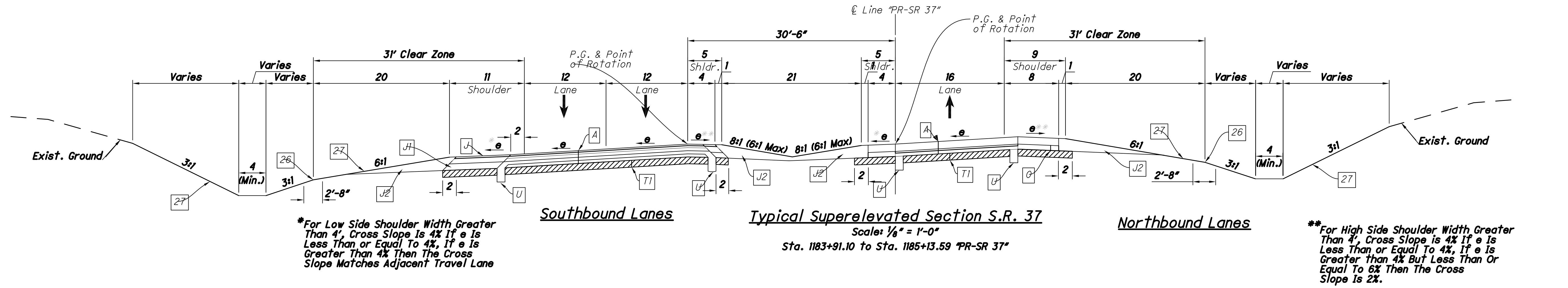
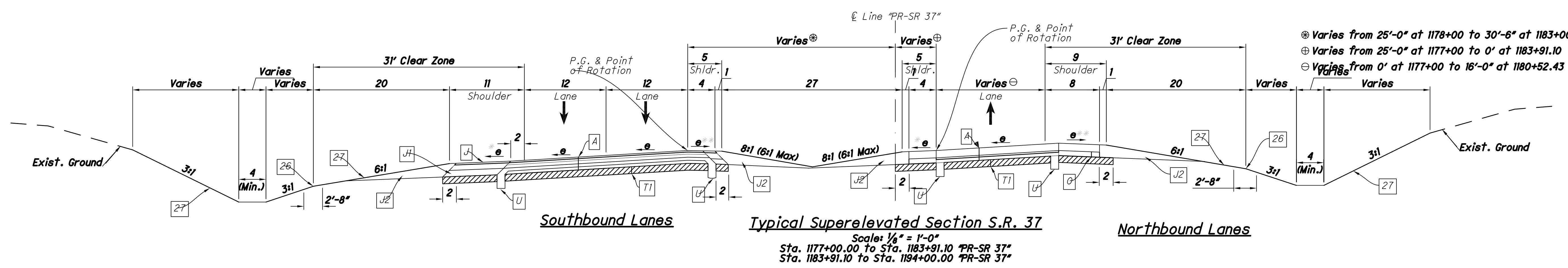
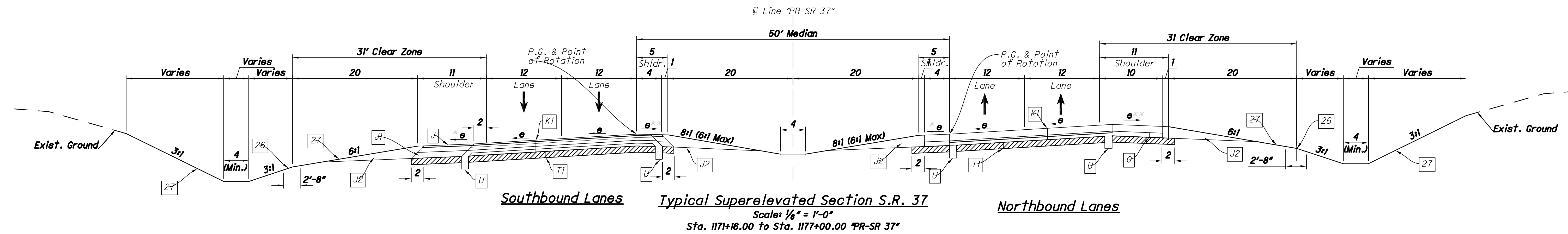
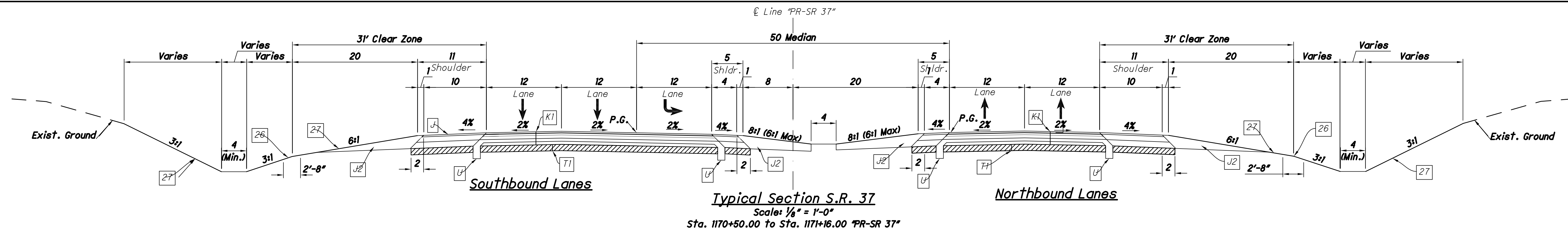
Note: All Shoulders On I-69 Shall Have Milled Shoulder Corrugations, Per INDOT STD. DWG. E606-SHCG-01 E606-SHCG-02 E606-SHCG-03

- A1 QC/QA, 8.5" on Subbase For PCCP (D-1 Joints @ 15' O.C. w/1.5" Dia. Dowel Bars) or 165 lbs/Sys QC/QA-HMA, 1, 64, Surface, 9.5 mm, on 275 lbs/Sys QC/QA-HMA, 1, 64, Intermediate, 19.0 mm, on 250 lbs/Sys QC/QA-HMA, 5, 76, Intermediate, OG 19.0 mm, on 385 lbs/Sys QC/QA-HMA, 1, 64, Base, 19.0 mm
- J2 Compacted Aggregate, No. 53, Variable Depth
- 0 8" Compacted Aggregate, No. 53, Base
- T1 Subgrade Treatment, Type IA
- T2 Subgrade Treatment, Type IIA
- T3 Subgrade Treatment, Type IIIA
- U 6" Underdrain
- 10 Guardrail
- 26 Sodding
- 27 Seeding, R

RECOMMENDED FOR APPROVAL	
DESIGNED: MDO	DRAWN:
CHECKED: HCF	CHECKED: MDO

INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS INTERCHANGE RAMPS	

HORIZONTAL SCALE	BRIDGE FILE	
AS NOTED	N/A	
VERTICAL SCALE	DESIGNATION	
AS NOTED	1006075	
SURVEY BOOK	PAGE	SHEETS
ELECTRONIC / AERIAL	TY-04	6 of 53
CONTRACT	PROJECT	
IR-33742	1006075	



- (K1) Full Depth HMA Pavement or QC/QA PCCP
- (J2) 8" Compacted Aggregate, No. 53
- (J3) Full Depth HMA Shoulder
- (0) 8" Compacted Aggregate, No. 53, Base
- (T1) Subgrade Treatment, Type IA
- (T2) Subgrade Treatment, Type IIA
- (T3) Subgrade Treatment, Type IIIA
- (U) 6" Underdrain
- (10) Guardrail
- (26) Sodding
- (27) Seeding, R

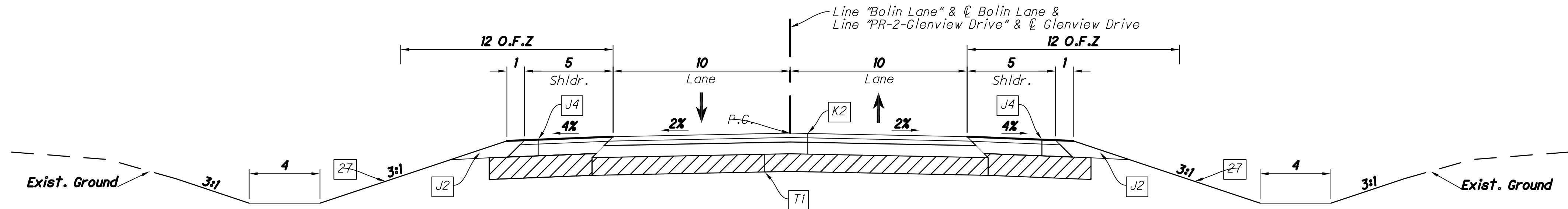
*For Low Side Shoulder Width Greater Than 4', Cross Slope Is 4% If e Is Less Than or Equal To 4%, If e Is Greater Than 4% Then The Cross Slope Matches Adjacent Travel Lane

**For High Side Shoulder Width Greater Than 4', Cross Slope Is 4% If e Is Less Than or Equal To 4%, If e Is Greater Than 4% But Less Than Or Equal To 6% Then The Cross Slope Is 2%.

RECOMMENDED FOR APPROVAL	
DESIGNED: MDO	DRAWN: KCH
CHECKED: HCF	CHECKED: MDO

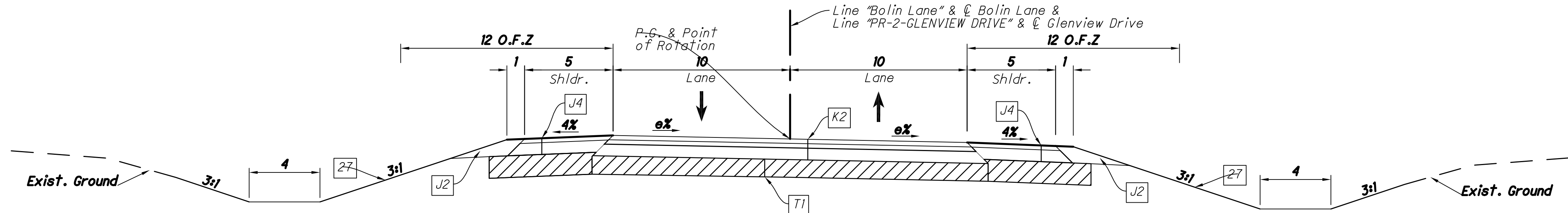
INDIANA DEPARTMENT OF TRANSPORTATION	
TYPICAL SECTIONS	
S.R. 37 - LINE "PR-SR 37"	

HORIZONTAL SCALE	BRIDGE FILE
AS NOTED	N/A
VERTICAL SCALE	DESIGNATION
AS NOTED	1006075
SURVEY BOOK	PAGE
ELECTRONIC / AERIAL	TY-05
CONTRACT	PROJECT
IR-33742	1006075



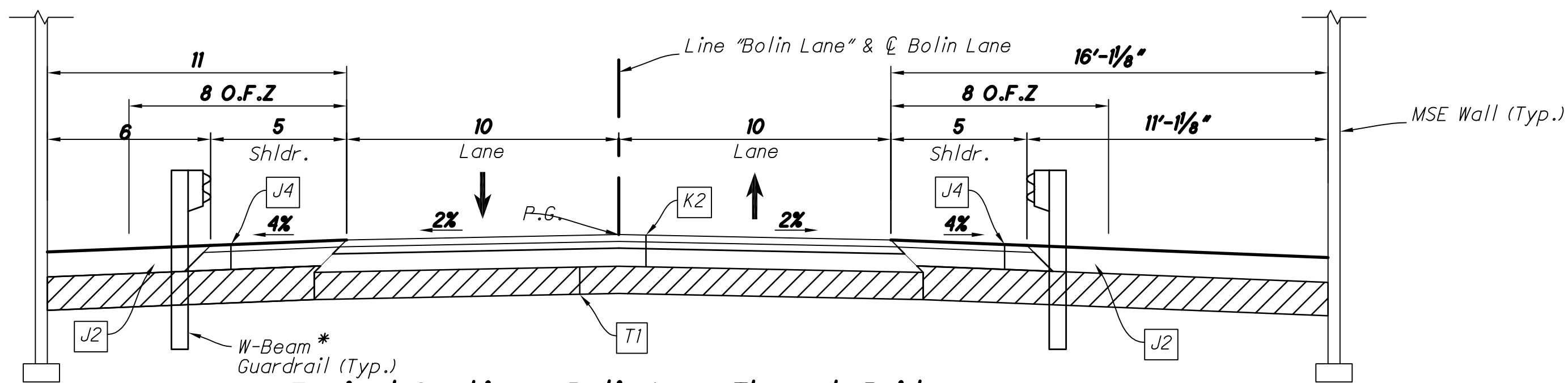
Typical Section - Bolin Lane & Glenview Drive

Scale: 1/4" = 1'-0"
Sta. 17+50.00 to Sta. 19+30.44 "Bolin Lane"
Sta. 20+95.56 to Sta. 22+40.00 "Bolin Lane"
Sta. 2+00.00 to Sta. 2+94.00 "PR-2-Glenview Drive"
Sta. 6+81.00 to Sta. 13+54.26 "PR-2-Glenview Drive"



Typical Superelevated Section - Glenview Drive

Scale: 1/4" = 1'-0"
Sta. 2+94.00 to Sta. 6+81.00 "PR-2-GLENVIEW DRIVE", e_{max} = 4.0% (RT)



Typical Section - Bolin Lane Through Bridge

Scale: 1/4" = 1'-0"
Sta. 19+30.44 to Sta. 20+95.56 "Bolin Lane"

* W-Beam Guardrail shall be used to protect vehicles on steep grade of Bolin Lane from MSE Wall Abutments

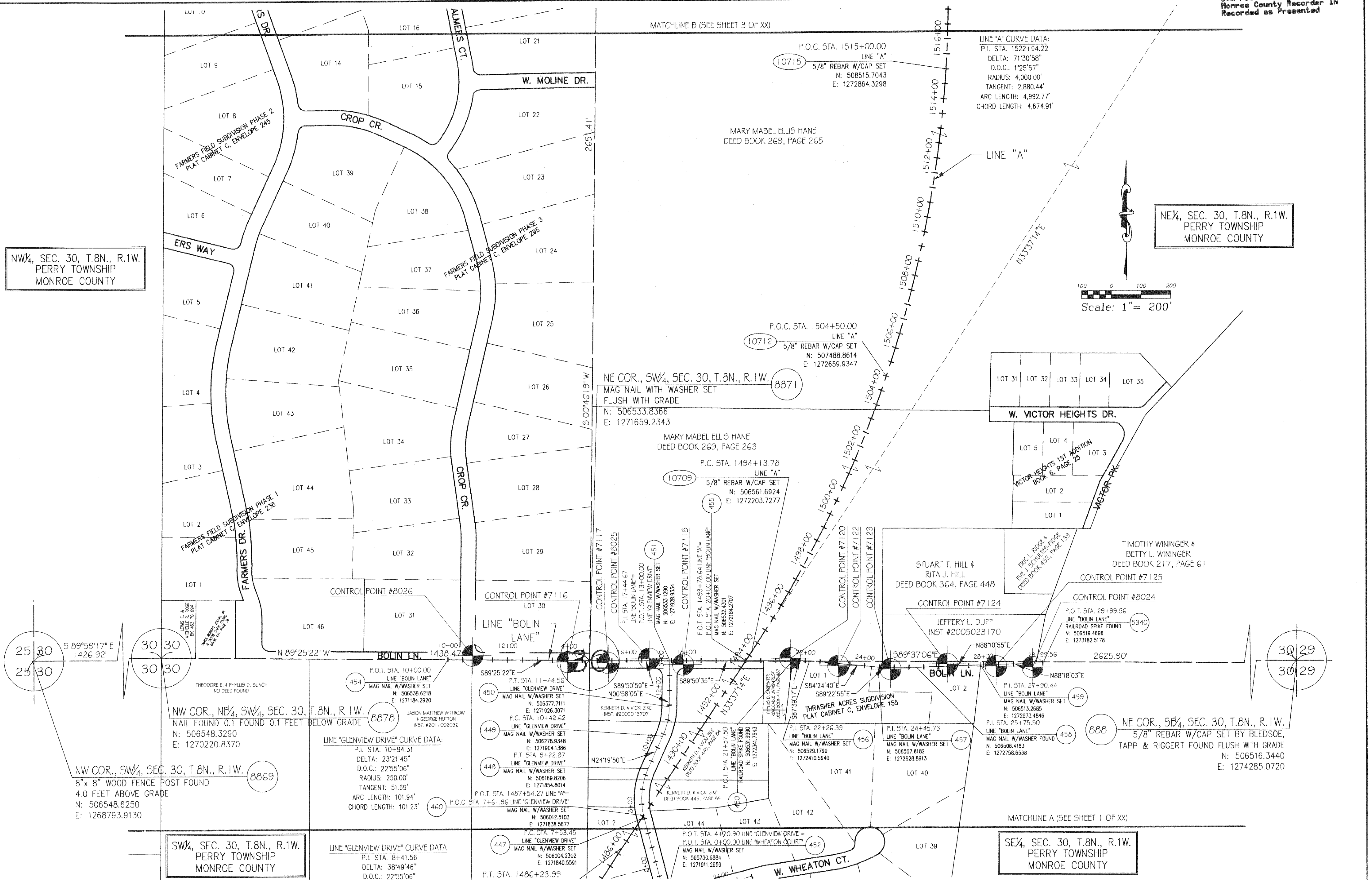
- (K2) Full Depth HMA Pavement
- (J2) 8" Compacted Aggregate, No. 53
- (J4) Full Depth HMA Shoulder
- (0) 8" Compacted Aggregate, No. 53, Base
- (T1) Subgrade Treatment, Type IA
- (T2) Subgrade Treatment, Type IIA
- (T3) Subgrade Treatment, Type IIIA
- (U) 6" Underdrain
- (10) Guardrail
- (26) Sodding
- (27) Seeding, R

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER	DATE
DESIGNED: MDO	DRAWN: KCH
CHECKED: HCF	CHECKED: MDO

INDIANA
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
BOLIN LANE & GLENVIEW DRIVE

HORIZONTAL SCALE	BRIDGE FILE	
AS NOTED	N/A	
VERTICAL SCALE	DESIGNATION	
AS NOTED	1006075	
SURVEY BOOK	PAGE	SHEETS
ELECTRONIC / AERIAL	TY-06	8 of 53
CONTRACT	PROJECT	
IR-33742	1006075	

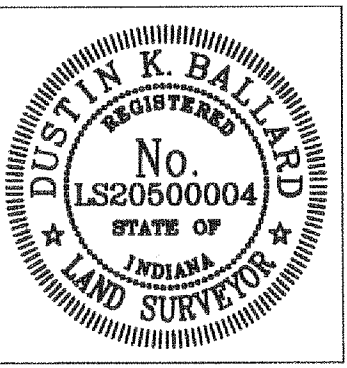


JOB NUMBER	101079
SURVEY STARTED	MAY 18, 2011
SURVEY COMPLETED	JUNE 21, 2011
ROUTE PLAT SHEETS	2 OF 8

SURVEYOR STATEMENT
THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF IBCS L.C. 1-12-20 REGARDING ROUTE SURVEYS EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF THE EXISTING PARCELS IS NOT A PART OF THIS SURVEY.

PREPARED BY:
BLN Beam, Longest and Neff, LLC
Consulting Engineers, Land Surveyors,
Land Planners & Landscape Architects

8126 Castleton Road, Indianapolis, IN 46250
Phone: 317.849.5832 www.B-L-N.com Fax: 317.841.4281

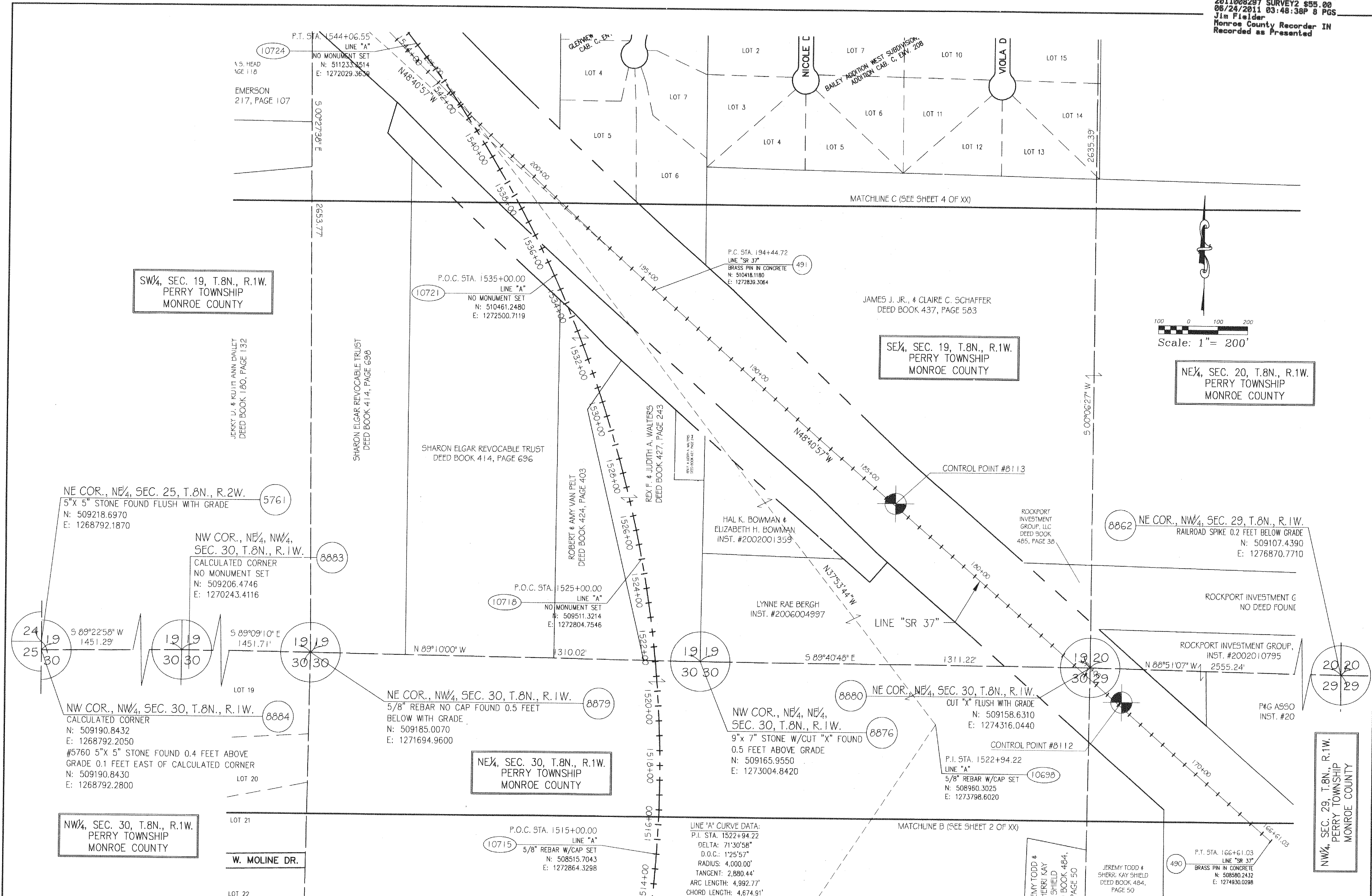


PREPARED BY: <u>Dustin K. Ballard</u>	6/22/11
REGISTERED LAND SURVEYOR	DATE
DRAWN: <u>JKR</u>	
CHECKED: <u>EJS</u>	

**I-69 SECTION 4, SEGMENT 9
EAST ZONE MONROE COUNTY**

LOCATION CONTROL ROUTE SURVEY

HORIZONTAL SCALE	BRIDGE FILE
1" = 200'	
COUNTY	DESIGNATION NO.
MONROE	
SURVEY BOOK	SHEETS
	of
CONTRACT	PROJECT NO.

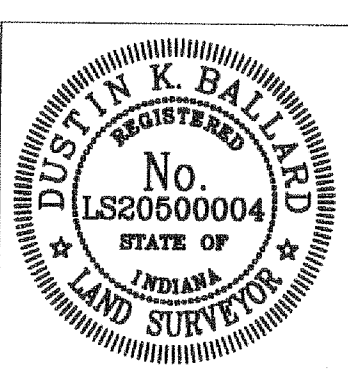


JOB NUMBER		
101079		
SURVEY STARTED		
MAY 18, 2011		
SURVEY COMPLETED		
JUNE 21, 2011		
ROUTE PLAT SHEETS		
3	OF	8

SURVEYOR STATEMENT
THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF 365 I.A.C. 1-12-20 THROUGH 1-12-25 REGARDING ROUTE SURVEYS EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF THE EXISTING PARCELS IS NOT A PART OF THIS SURVEY.

PREPARED BY:
BLN Beam, Longest and Neff, LLC
Consulting Engineers, Land Surveyors,
Land Planners & Landscape Architects

8126 Castleton Road, Indianapolis, IN 46250
Phone: 317.849.5832 Fax: 317.841.4281



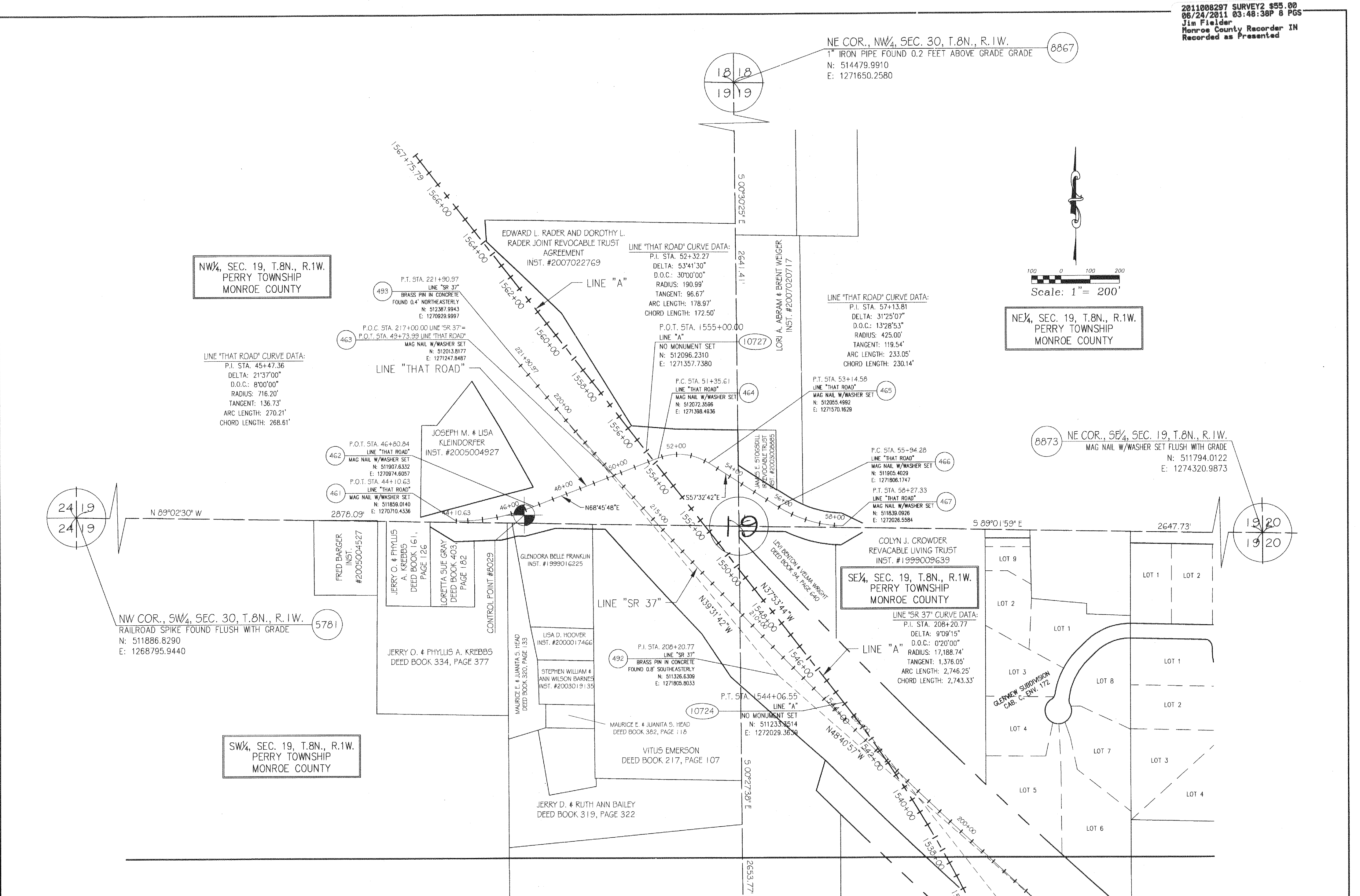
PREPARED BY: Dustin K. Ballard 6/22/11
REGISTERED LAND SURVEYOR DATE

DRAWN: JKR
CHECKED: EJS

**I-69 SECTION 4, SEGMENT 9
EAST ZONE MONROE COUNTY**

LOCATION CONTROL ROUTE SURVEY

HORIZONTAL SCALE	BRIDGE FILE
1" = 200'	
COUNTY	DESIGNATION NO.
MONROE	
SURVEY BOOK	SHEETS
	of
CONTRACT	PROJECT NO.



JOB NUMBER 101079	SURVEYOR STATEMENT <small>THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF IBS I.A.C. 1-12-20 THROUGH 1-12-25 REGARDING ROUTE SURVEYS, EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF THE EXISTING PARCELS IS NOT A PART OF THIS SURVEY.</small> PREPARED BY: BLN Beam, Longest and Neff, LLC <small>Consulting Engineers, Land Surveyors, Land Planners & Landscape Architects</small> <small>8126 Castleton Road, Indianapolis, IN 46250 Phone: 317.849.5832 www.B-L-N.com Fax: 317.841.4281</small>	DUSTIN K. BALLARD REGISTERED No. LS20500004 STATE OF INDIANA LAND SURVEYOR	PREPARED BY: <u>Dustin K. Ballard</u> 6/22/11 REGISTERED LAND SURVEYOR DATE	I-69 SECTION 4, SEGMENT 9 EAST ZONE MONROE COUNTY LOCATION CONTROL ROUTE SURVEY	HORIZONTAL SCALE 1" = 200'	BRIDGE FILE
SURVEY STARTED MAY 18, 2011					COUNTY MONROE	DESIGNATION NO.
SURVEY COMPLETED JUNE 21, 2011					SURVEY BOOK	SHEETS of
ROUTE PLAT SHEETS 4 OF 8					CONTRACT	PROJECT NO.

SURVEYOR'S REPORT

PURPOSE:
The purpose of this survey was to prepare a Location Control Route Survey Plat for the establishment of the centerline of I-69 within Segment 8 of the Interstate 69, Tier 1, Preferred Alternative 3C in Perry Township, Monroe County, Indiana.

SCOPE:
The Route Survey begins at the South Line of the Southwest Quarter of Section 30, Township 8 North, Range 1 West and proceeds northeasterly, northerly and northwesterly to the Northwest Quarter of Section 19, Township 8 North, Range 1 West at which point I-69 merges with S.R. 37.

FIELD MEASUREMENTS:
Field measurements for this survey were made in accordance with specifications outlined in 865 I.A.C. 1-12-22. The decimal places shown hereon for measurements and coordinates do not necessarily indicate the precision of the work, but to allow for closure and adjustment by others if desired.

BASIS OF BEARINGS:
Bearings for this survey are based on the “I-69 Coordinate System, East Zone” grid (provided by Bernardin, Lochmueller and Associates, Inc.), as defined below. Unless noted otherwise, all measured and calculated distances and coordinates shown hereon are also based on said grid per the NAD 89 (1997) adjustment and are reported in U.S. Survey Feet. The “I-69 Coordinate System, East Zone” is a transverse Mercator projection of the North American Datum of 1983, having a central meridian of 86 degrees 25 minutes 12 seconds west of Greenwich, on which meridian the scale is set to 1.0000260. The origin of grid coordinates is at the intersection of said meridian and the parallel 38 degrees 09 minutes 36 seconds north latitude. This origin is given the coordinates: North=50,000 meters and East=400,000 meters. This projection was designed to minimize the difference between ground measured distances and grid coordinate inverses specific to Segment 8 of the Interstate 69, Tier 1, Preferred Alternative 3C in southwestern Indiana. Controlling monuments used for this survey are numbers 60-65 provided by Bernardin, Lochmueller and Associates, Inc. as shown hereon. Static and RTK observations were used to establish the control for this survey. Said control was adjusted using a least squares analysis software.

ALIGNMENTS:

Line “A” was provided by Bernardin, Lochmueller and Associates, Inc. and is the proposed centerline of Interstate 69.

Line “SR 37” was established per information from Indiana State Highway Commission plans for STF-Project No. 893 (8) R/W, const. & utilities. Monuments from said plans were located at stations P.T. 166+61.04 and P.C. 194+44.72 and checked to within hundredths of a foot when compared to plan stationing. A monument was found at P.I. station 208+20.78 and was found to be 0.8 feet short when compared to the corresponding curve data. However, a best fit line was created using the three above mentioned monuments and was found to be off by less than a tenth of a foot. The station of 194+44.72 was held at the corresponding P.C. and run forward and back from said point. The degree of curvature from the plans was held to establish the curve at said P.C. and misses a monument found at the P.T. station 221+90.07 by approximately 0.4 feet in a southwesterly direction.

Line “That Road” was established per information from Indiana State Highway Commission plans for STF-Project No. 893 (8) R/W, const. & utilities. A station equation of 217+00 “A” = 49+73.99 “That Road” was held to establish stationing. When holding the angular relationship between the centerlines of Line “A” (S.R. 37) and Line “That Road” it did not fit well with the existing pavement of That Road and therefore Line “That Road” was rotated to fit the existing pavement.

Line “Bolin Lane” is defined by the North Line of the Southwest Quarter of Section 30, Township 8 North, Range 1 West for its western portion. For its eastern portion, it is defined by a combination of found monuments and deed lines as it runs along the asphalt roadway of Bolin Lane. Line “Bolin Lane” is not meant to represent the physical centerline of Bolin Lane as it exists during the time of this survey.

Line “Glenview Drive” was established per geometry from the plat of Rolling Glen Subdivision Phase 3, Final Plat as recorded in Plat Cabinet C, Envelope 339.

Line “Wheaton Court” was established per geometry from the plat of Rolling Glen Subdivision Phase 3, Final Plat as recorded in Plat Cabinet C, Envelope 339.

In accordance with Title 865, Article 1.1, Chapter 12, Sections 20 through 25 of the Indiana Administrative Code, the following observations and opinions are submitted regarding the cause and amount of uncertainties in the lines and corners found or established by this survey because of any of the following:

AVAILABILITY AND CONDITION OF REFERNECE MONUMENTS:
Many of the monuments set marking the corners during the United States Public Lands Surveys conducted in Indiana have been lost or obliterated and have not been perpetuated, or replaced, over the time since said surveys were performed. Therefore, great care was taken to re-establish said corners using as much information and evidence as possible to restore each monument to its original location as accurately as can be determined. Any uncertainty with these re-established monuments will be shown hereon. Any found monuments with their location being used to establish the controlling lines for this survey will be explained in reference to their origin (if able to be determined), size, type, character and relationship to the surface of the ground, roadway, etc. The origin of said monuments include, but are not limited to, federal and state highway plans, public lands survey notes, local surveys

both recorded and unrecorded, local landowner testimony, natural and manmade topographical features and information obtained from local county offices such as deeds, plats, etc...

All set “REBAR” are 24” #5 rebar with orange plastic cap stamped “B-L-N - RANDOM POINT”.

RECORD DOCUMENTATION USED:
Indiana State Highway Commission plans for STF-Project No. 893 (8) R/W, const. & utilities (plans for State Road 37), a survey for John Elgar performed by Steven Archer (Instrument Number 2004001228), a survey for Mark Freeman performed by Bledsoe, Tapp & Riggert, Inc. (Instrument Number 2005018456), a survey for Rodney Van Pelt performed by Eric Deckard (Instrument Number 2008012094), a survey for Edward Rader performed by Eric Deckard (Instrument Number 2007008907), a survey for Sarah Bock performed by Eric Deckard (Instrument Number 2009008880), a survey for Brent Weiger performed by Eric Deckard (Instrument Number 2008012095), a survey for Timothy J. Hightower performed by Bledsoe Tapp & Riggert, Inc. (Instrument Number 2004013529), a survey for Joseph Kleindorfer performed by Bledsoe Tapp & Riggert, Inc. (Instrument Number 2004010134), a survey for Michael Harding performed by Bledsoe Tapp & Riggert, Inc. (Instrument Number 2003020473), a survey for Michael Harding performed by Bledsoe Tapp & Riggert, Inc. (Instrument Number 2003020475), a survey performed by Philip O. Tapp & Company, Inc. (Instrument Number 2011001881), a survey for Kenneth Zike performed by Bledsoe Tapp & Riggert, Inc. (Cabinet S, Envelope 109), a survey for Kenneth Eads performed by Bledsoe Tapp & Riggert, Inc. (Cabinet S, Envelope 95), a survey for the Emmanuel Baptist Church performed by Bledsoe Tapp & Riggert, Inc. (Book 5, page 220), a survey for William R. Polley performed by Stephen E. Ramsey (Book 4, page 566), a survey for Rex Walters performed by Steven W. Archer (Book 4, page 229), Bailey Addition-West Section (Plat Cabinet C, Envelope 106), Bailey Addition west Subdivision Second Addition (Plat Cabinet C, Envelope 208), Glenview Subdivision (Plat Cabinet C, Envelope 172), Farmers Field Subdivision Phase 1 (Plat Cabinet C, Envelope 236), Farmers Field Subdivision Phase 2 (Plat Cabinet C, Envelope 245), Farmers Field Subdivision Phase 3 (Plat Cabinet C, Envelope 295), Rolling Glen Subdivision Phase 1 (Plat Cabinet C, Envelope 252), Rolling Glen Subdivision Phase 2 (Instrument Number 2000018124), Rolling Glen Subdivision Phase 3 (Instrument Number 2003019770), Thrasher Acres Subdivision (Plat Cabinet C, Envelope 155), Robinson Industrial Park Subdivision Phase One (Plat Cabinet C, Envelope 283), Victor Heights 1st Addition (Book 6, page 25), the plat of Amended Eagle View Section One subdivision (Plat Cabinet C, Envelope 157)

Section 30, Township 8 North, Range 1 West:

Point #8852 - The southwest corner of the Southwest Quarter of Section 30 - Calculated corner - A 5/8” rebar with a plastic cap was set 0.4 feet above grade by Beam, Longest and Neff was placed 20.46 feet (31 links per the GLO notes for this corner) south of the northeast corner (Point #8853) of Section 36, Township 8 North, Range 2 West, and on the East Line of the Northeast Quarter of said Section 36. Per a survey titled “Hacker Administrative Subdivision” (Instrument Number 2000008067) performed by Bledsoe, Tapp & Riggert, Inc. Based on the references found per its' Monroe County Surveyors Office Monument Record sheet, there may be as much as 0.7 feet of uncertainty associated with this corner.

Point #8848 - The southwest corner of the Southeast Quarter of Section 30 - An 8” diameter wooden fence post was found 4.5 feet above grade per a survey for Mark Freeman (Instrument Number 2005018456) performed by Bledsoe, Tapp & Riggert, Inc. No Monroe County Surveyors Office Monument Record sheet exists for this corner. The uncertainty associated with this corner is unknown.

Point #5778 - The southeast corner of the Southeast Quarter of Section 30 - A 5/8” rebar with a plastic cap set by Smith Neubecker and Associates was found flush with grade per the Monroe County Surveyors Office Monument Record sheet. There is considered to be no positional uncertainty associated with this corner.

Point #8869 - The northwest corner of the Southwest Quarter of Section 30 - An 8”x 8” wooden fence post was found 4.0 feet above grade per a survey for Mark Freeman (Instrument Number 2005018456) performed by Bledsoe, Tapp & Riggert, Inc. No Monroe County Surveyors Office Monument Record sheet exists for this corner. The uncertainty associated with this corner is unknown.

Point #8878 - The northwest corner of the Northeast Quarter of the Southwest Quarter of Section 30 - A small nail was found 0.1 feet below grade per a survey for Kenneth Eads performed by Bledsoe Tapp & Riggert, Inc. (Cabinet S, Envelope 95). No Monroe County Surveyors Office Monument Record sheet exists for this corner. Based on said survey, the uncertainty associated with this corner is approximately 0.8 feet.

Point #8871 - The northeast corner of the Southwest Quarter of Section 30 - A mag nail with washer was set to replace a “PK Nail” per a survey for Kenneth Zike performed by Bledsoe Tapp & Riggert, Inc. (Cabinet S, Envelope 109). A line was run between the fence post held as the South Quarter corner of said section and a rebar found on the West Line of said Southwest Quarter per said survey. Said line was then projected northerly 202.00' feet (distance from the survey) to establish this corner. No Monroe County Surveyors Office Monument Record sheet exists for this corner. Per the record distance from said survey and the measured distance between said monuments, there may be as much as 0.7 feet of uncertainty associated with this corner.

Point #8881 - The northeast corner of the Southeast Quarter of Section 30 - A 5/8” rebar with a plastic cap set by Bledsoe, Tapp & Riggert, Inc. was found flush with grade per the plat of Robinson Industrial Park Subdivision Phase One (Plat Cabinet C, Envelope 283). Said plat shows this rebar as being found with lines intersecting at this monument and running northerly and easterly from said monument that are labeled “W. Line NW ¼, Sec. 29” and “S. Line NW ¼, Sec. 29”, respectively. However, no note exists stating that this monument represents this corner. I believe the intent to be that the rebar does in fact represent said corner. No Monroe County Surveyors Office Monument Record sheet exists for this corner. A fence post was found approximately 32.4 feet westerly of this

monument with an ancient fence running northerly and easterly from it. Based on the position of the fence post and fence lines, there may be as much as 32.4 feet of uncertainty with this corner.

Point #8884 - The northwest corner of the Northwest Quarter of Section 30 - Calculated corner - A 5”x 5” stone found 0.4 feet above grade per the Monroe County Surveyors Office Monument Record sheet. Said stone was found approximately 0.1 feet easterly of a line run between a stone found (Point #5761) at the northeast corner of the Northeast Quarter of Section 25, Township 8 North, Range 2 West and the wood post (Point #8869) held as the northwest corner of the Southwest Quarter of Section 30. A line was run between the northwest corner of the Northeast Quarter of the Northwest Quarter of Section 30 (Point #8883) and said stone. Said line was then extended westerly to its' intersection with the East Line of the Northeast Quarter of Section 25, Township 8 North, Range 2 West, thus creating the northwest corner of the Northwest Quarter of Section 30 (Point #8884). There is considered to be no positional uncertainty associated with this corner.

Point #8879 - The northeast corner of the Northwest Quarter of Section 30 - A 5/8” rebar (no cap) was found 0.5 feet below grade - No Monroe County Surveyors Office Monument Record sheet exists for this corner. Said rebar is shown as being set per a survey for Kenneth Eads performed by Bledsoe Tapp & Riggert, Inc. (Cabinet S, Envelope 95). Based on the surveyed distance versus the record distance (from the above mentioned survey) of the East Line of the Northwest Quarter of said Section, there may be as much as 0.7 feet of uncertintcy associated with this corner.

Point #8883 - The northwest corner of the Northeast Quarter of the Northwest Quarter of Section 30 - Calculated corner, no monument set - No Monroe County Surveyors Office Monument Record sheet exists for this corner. This corner was created using geometry from the plats of Farmers Field Subdivision Phase 1 (Plat Cabinet C, Envelope 236), Farmers Field Subdivision Phase 2 (Plat Cabinet C, Envelope 245), Farmers Field Subdivision Phase 3 (Plat Cabinet C, Envelope 295). The uncertainty associated with this corner is unknown.

Point # 8876 - The northwest corner of the Northeast Quarter of the Northeast Quarter of Section 30 - A 9”x 7” stone with a cut “X” was found 0.5 feet above grade per a survey for Rex Walters performed by Steven W. Archer (Book 4, page 229). No Monroe County Surveyors Office Monument Record sheet exists for this corner. The uncertainty associated with this corner is unknown.

Point # 8880 - The northeast corner of the Northeast Quarter of Section 30 - A cut “X” was found in the passing (left) lane of the northbound travel lanes of S.R. 37. A railroad spike is identified as representing this corner per a survey for the Emmanuel Baptist Church performed by Bledsoe Tapp & Riggert, Inc. (Book 5, page 220). Said cut “X” fit well with other monuments located per said survey. Said cut “X” is identified as being the northeast corner of the Northeast Quarter of Section 30 corner per the plat of Robinson Industrial Park Subdivision Phase One (Plat Cabinet C, Envelope 283). However, per the Indiana State Highway Commission plans for STF-Project No. 893 (8) R/W, const. & utilities (plans for State Road 37) this corner is located at Station 175+50, 19 feet left of Line “A” (centerline for State Road 37). The position from the plans is approximately 67 feet westerly of said cut “X”. Furthermore it appears that the fence line running northerly from Point #8881 (the northeast corner of the Southeast Quarter of Section 30) would fit better with the position calculated from said plans. Also supporting the cut “X” are the plats for Bailey Addition-West Section (Plat Cabinet C, Envelope 106) and Bailey Addition west Subdivision Second Addition (Plat Cabinet C, Envelope 208) as well as other deeds and lines of possession north of S.R. 37. Due to the availability of record documentation in the form of surveys and plats, I chose to hold the cut “X”. No Monroe County Surveyors Office Monument Record sheet exists for this corner. There is considered to be as much as 67 feet of positional uncertainty associated with this corner.

Section 19, Township 8 North, Range 1 West:

Point #5781 - The northwest corner of the Southwest Quarter of Section 19 - A railroad spike was found flush with grade per a survey for Joseph Kleindorfer performed by Bledsoe Tapp & Riggert, Inc. (Instrument Number 2004010134). No Monroe County Surveyors Office Monument Record sheet exists for this corner. Per said survey the uncertainty associated with this corner is approximately 0.1 feet.

Point #5762 - The northeast corner of the Southwest Quarter of Section 19 - A 5/8” rebar (no cap) was found 0.2 feet below grade per a survey for Rodney Van Pelt performed by Eric Deckard (Instrument Number 2008012094). No Monroe County Surveyors Office Monument Record sheet exists for this corner. Per a survey for Joseph Kleindorfer performed by Bledsoe Tapp & Riggert, Inc. (Instrument Number 2004010134) the uncertainty associated with this corner is approximately 0.1 feet.

Point #8873 - The northeast corner of the Southeast Quarter of Section 19 - A mag nail with a washer was set to replace a “spike” as identified by its' Monroe County Surveyors Office Monument Record sheet exists for this corner. As the reference distances on said Monument Record sheet conflicted with one another, monuments located from the survey for the Emmanuel Baptist Church (adjoining to the southeast) performed by Bledsoe Tapp & Riggert, Inc. (Book 5, page 220) and the plats for Bailey Addition-West Section (Plat Cabinet C, Envelope 106) and Bailey Addition west Subdivision Second Addition (Plat Cabinet C, Envelope 208) were used to reestablish this corner. Based on record information from the above mentioned documentation, there may be as much as 0.6 feet of positional uncertainty associated with this corner.

Point #5763 - The northwest corner of the Northwest Quarter of Section 30 - A 3/4” iron pipe found 0.7 feet below grade per the Monroe County Surveyors Office Monument Record sheet. However, the road in this area has been improved and its' Monument Record sheet has not been updated. New distances were pulled using the same reference points for this survey. There is considered to be no positional uncertainty associated with this corner.

Point #8867 - The northeast corner of the Northwest Quarter of Section 30 - A 1” iron pipe was found 0.2 feet above grade per a survey for Rodney Van Pelt performed by Eric Deckard (Instrument Number 2008012094). A railroad spike was called for (a mag nail was found flush) per a survey for Joseph Kleindorfer performed by Bledsoe Tapp & Riggert, Inc. (Instrument Number 2004010134). The 1” iron pipe was held as it seemed to fit better with a fence and tree line running to the east. No Monroe County Surveyors Office Monument Record sheet exists for this corner. Based upon the location of the 1” iron pipe and the mag nail, the uncertainty associated with this corner is approximately 26.4 feet.

Point #8866 - The northeast corner of the Northeast Quarter of Section 30 - A 5/8” rebar was found 1.0 feet below grade per its' Monroe County Surveyors Office Monument Record sheet. There is considered to be no positional uncertainty associated with this corner.

Section 29, Township 8 North, Range 7 West:

Point #8865 - The southeast corner of the Southwest Quarter of Section 29 - A PK nail was found flush with grade per its' Monroe County Surveyors Office Monument Record sheet. There is considered to be no positional uncertainty associated with this corner.

Point #8864 - The southeast corner of the Southeast Quarter of Section 29 - A railroad spike was 0.4 feet below with grade per its' Monroe County Surveyors Office Monument Record sheet. There is considered to be no positional uncertainty associated with this corner.

Point #8882 - The northeast corner of the Southwest Quarter of Section 30 - A calculated corner with no monument set - A line-line intersection from corresponding quarter corners (point numbers 5231, 8865, 8881 and 8862) was created to establish this corner. No Monroe County Surveyors Office Monument Record sheet exists for this corner. The position of this corner fits well with a rock wall running along the North Line of the Southwest Quarter of this section and varies by as much as 2 feet.

Point #5231 - The northeast corner of the Southeast Quarter of Section 29 - A 0.3-foot diameter pipe was found 0.3 feet below with grade per its' Monroe County Surveyors Office Monument Record sheet. There is considered to be no positional uncertainty associated with this corner.

Point #8862 - The northeast corner of the Northwest Quarter of Section 29 - A railroad spike was 0.2 feet below grade per its' Monroe County Surveyors Office Monument Record sheet. There is considered to be no positional uncertainty associated with this corner.

Point #8863 - The northeast corner of the Northeast Quarter of Section 29 - A railroad spike was found 0.4 feet below grade. No Monroe County Surveyors Office Monument Record sheet exists for this corner. However, a yellow “Survey Marker” sign was posted on a utility pole on the north side of Church Lane. The uncertainty associated with this corner is unknown.

Section 20, Township 8 North, Range 1 West:

Point #8875 - The northeast corner of the Southwest Quarter of Section 20 - A mag nail with a washer was set flush with grade. No Monroe County Surveyors Office Monument Record sheet exists for this corner. However, a yellow “Survey Marker” sign was posted on a utility pole on the south side of That Road. This corner was reestablished per a survey for the Emmanuel Baptist Church performed by Bledsoe Tapp & Riggert, Inc. (Book 5, page 220). Based upon monuments found per the plat of Amended Eagle View Section One subdivision (Plat Cabinet C, Envelope 157) there may be as much as 7.7 feet of positional uncertainty associated with this corner.

Point #8868 - The northeast corner of the Southeast Quarter of Section 20 - A 5/8” rebar with a plastic cap set by Bledsoe, Tapp & Riggert, Inc. was found 0.7 feet above grade and held as the best evidence of this corner. No Monroe County Surveyors Office Monument Record sheet exists for this corner. The uncertainty associated with this corner is unknown.

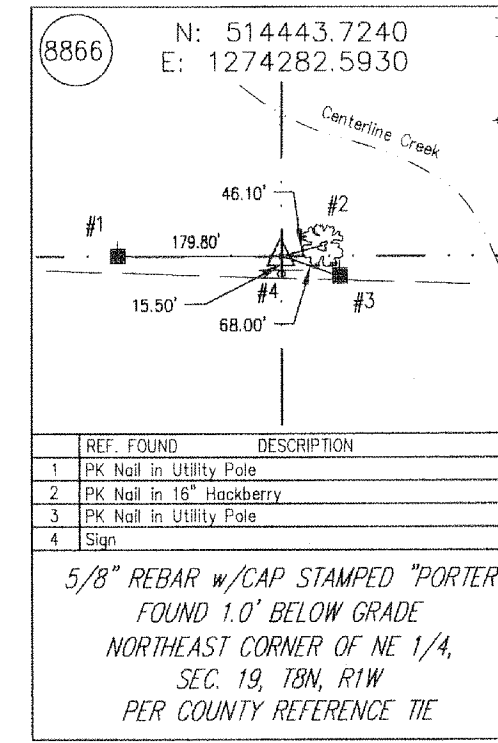
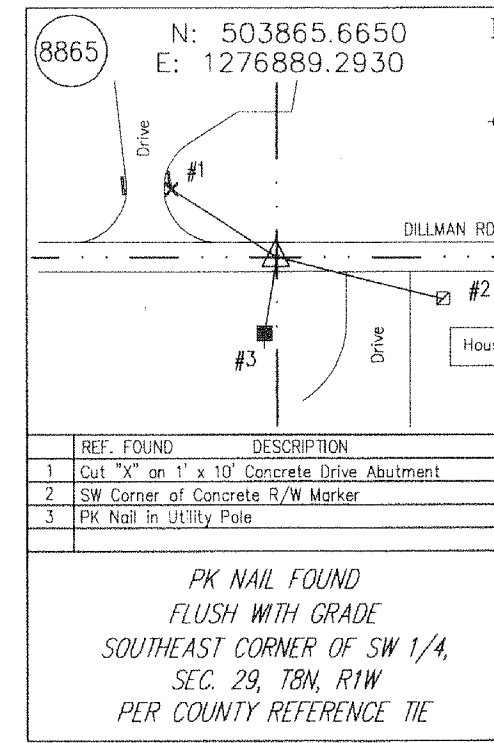
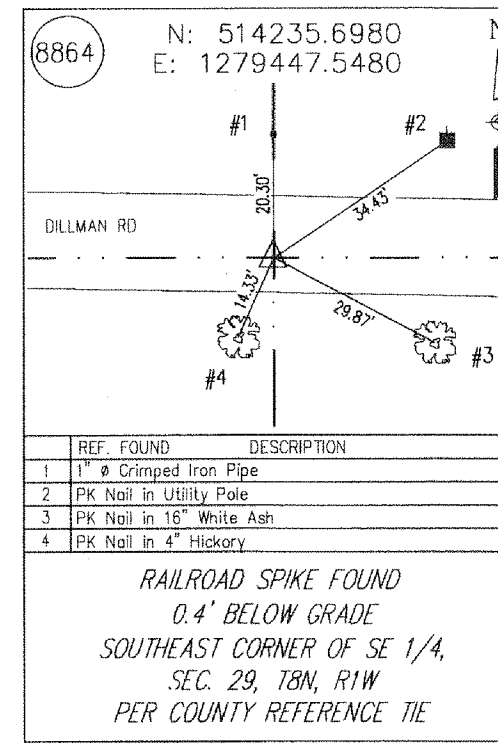
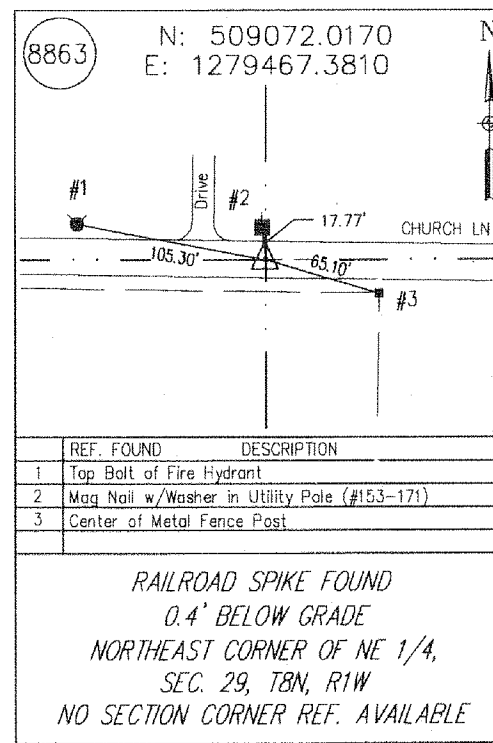
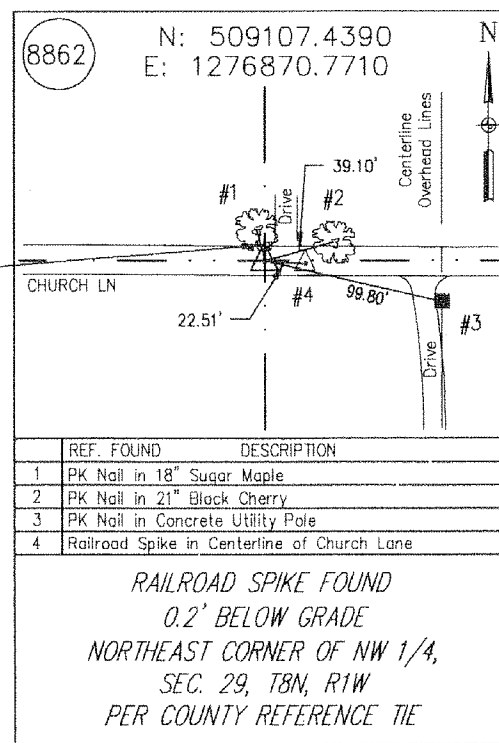
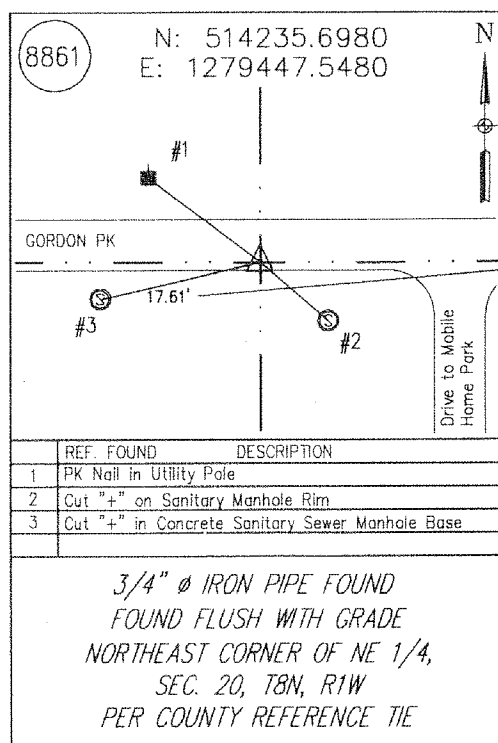
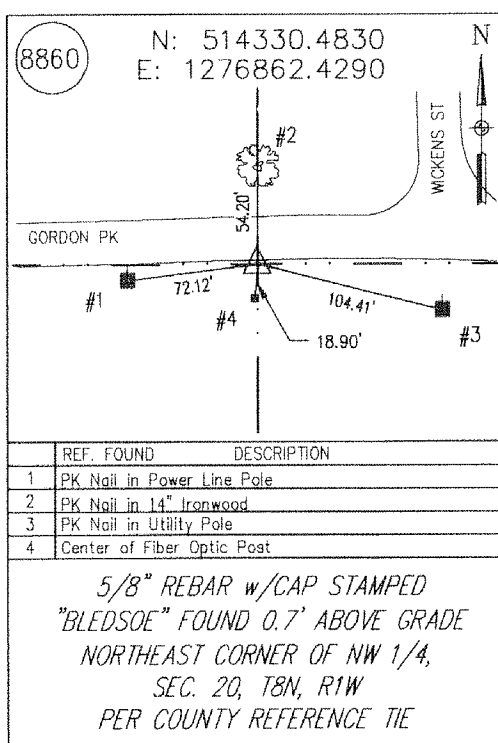
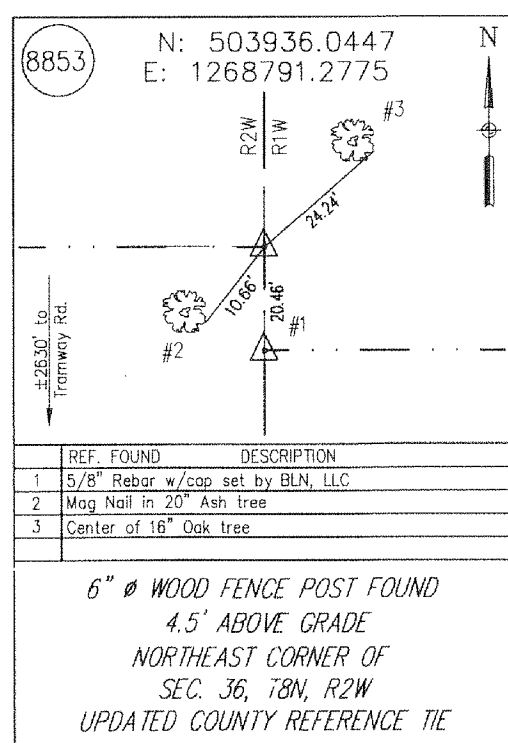
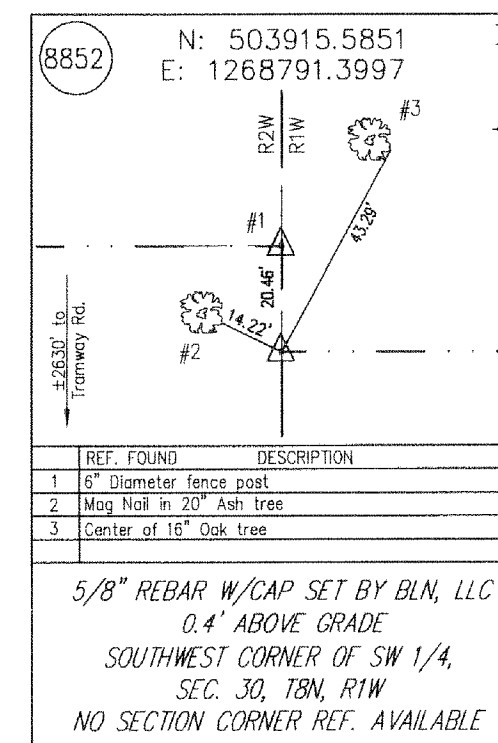
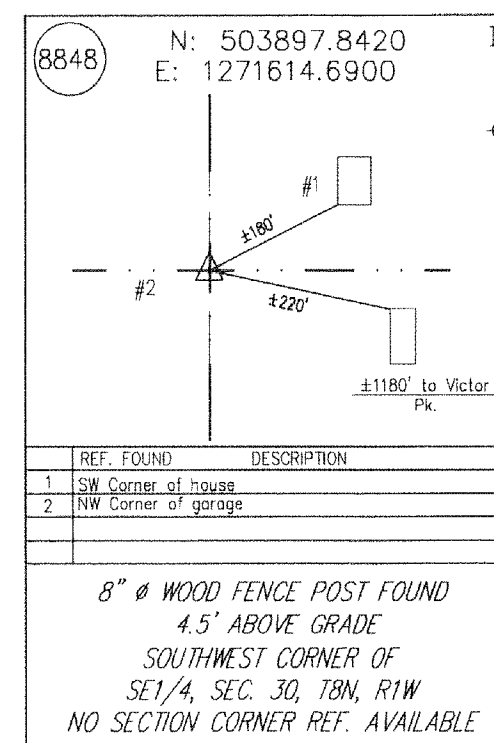
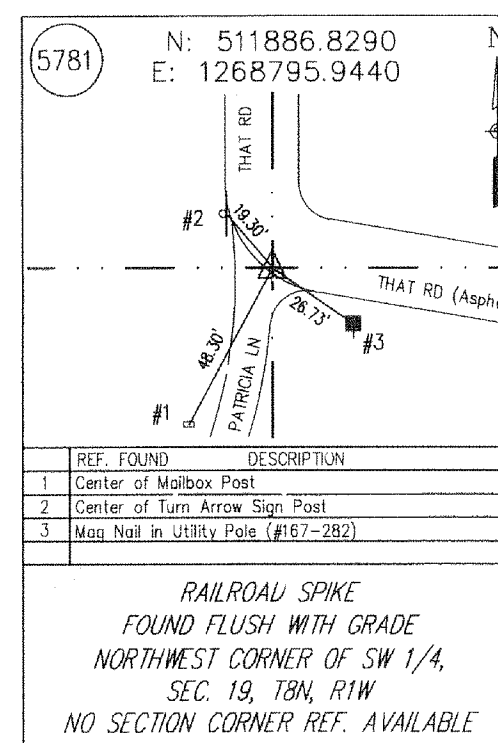
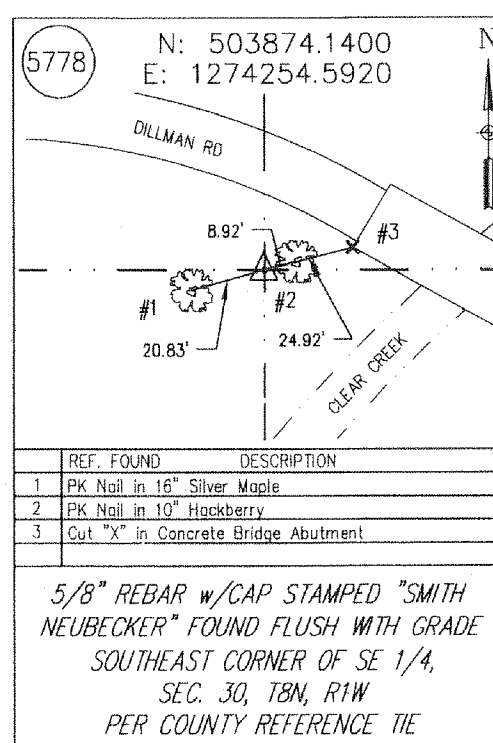
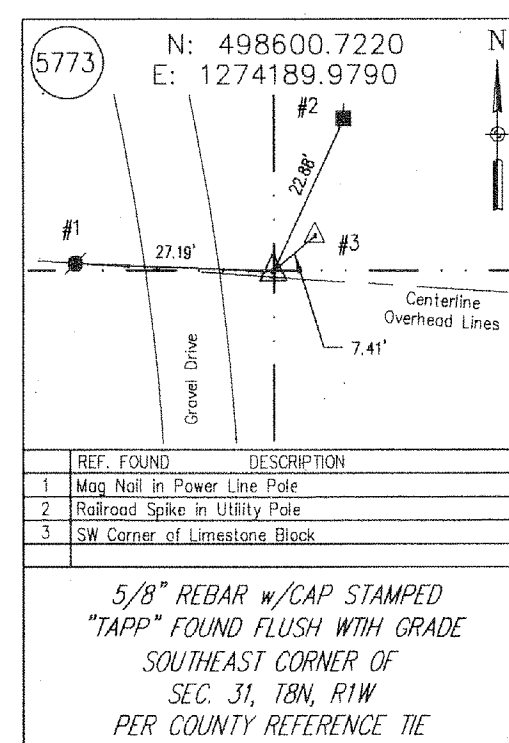
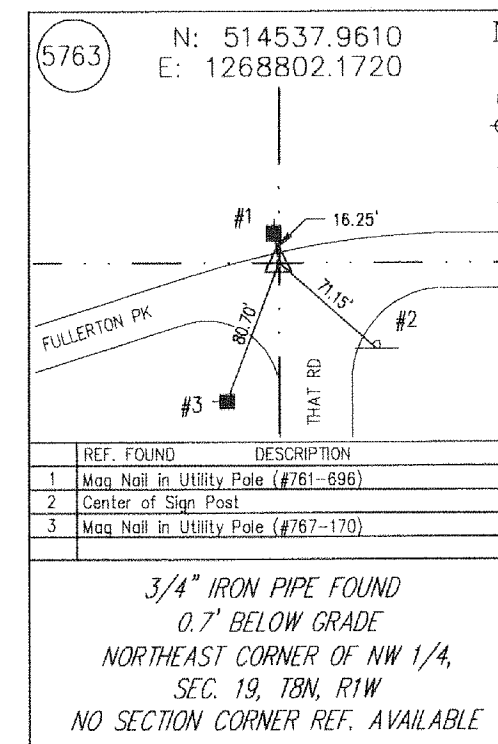
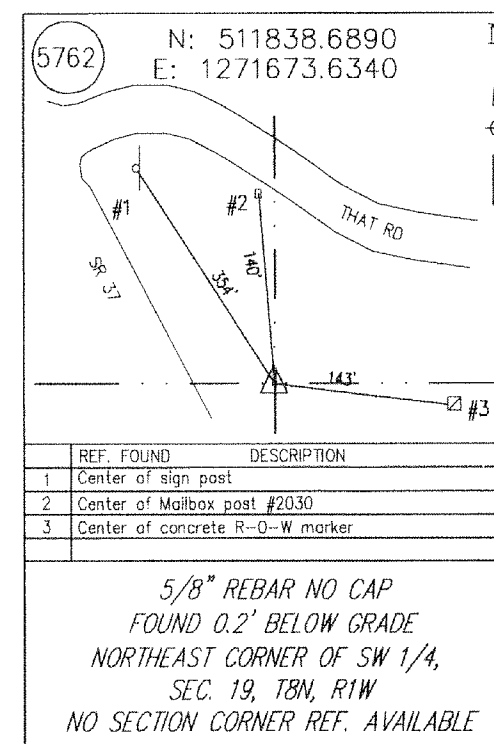
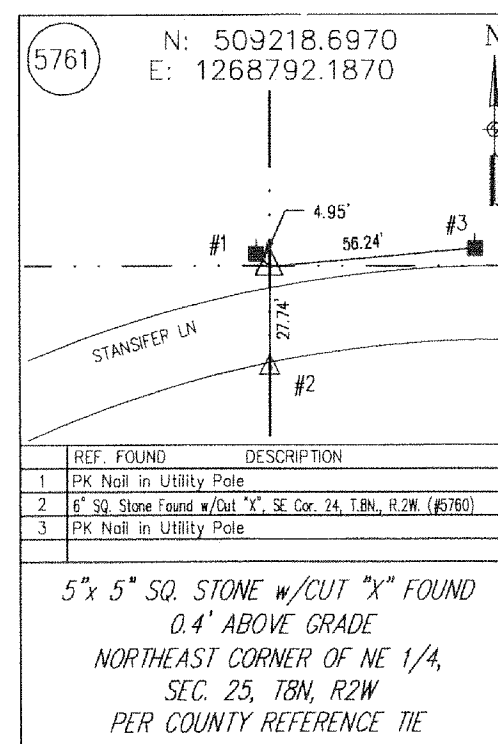
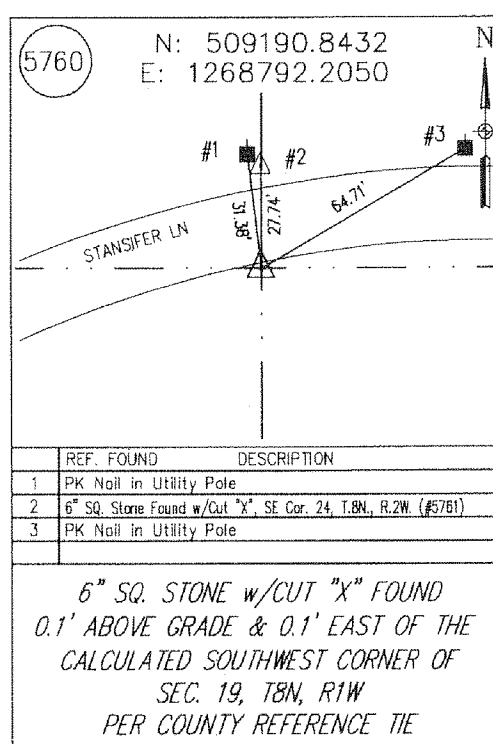
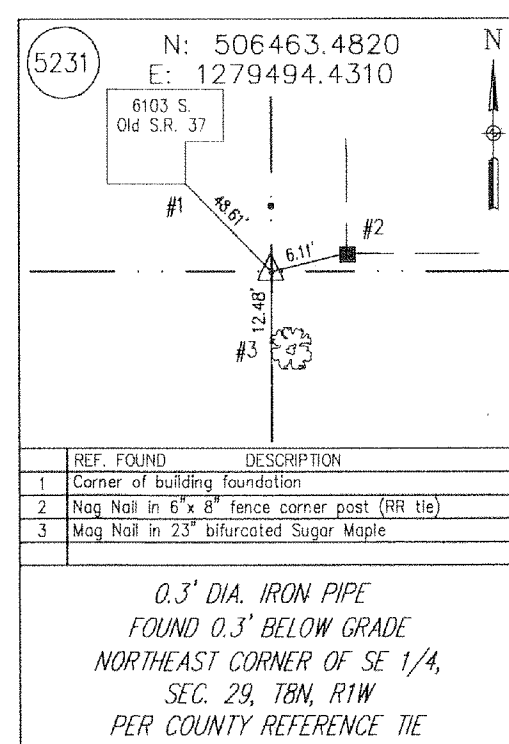
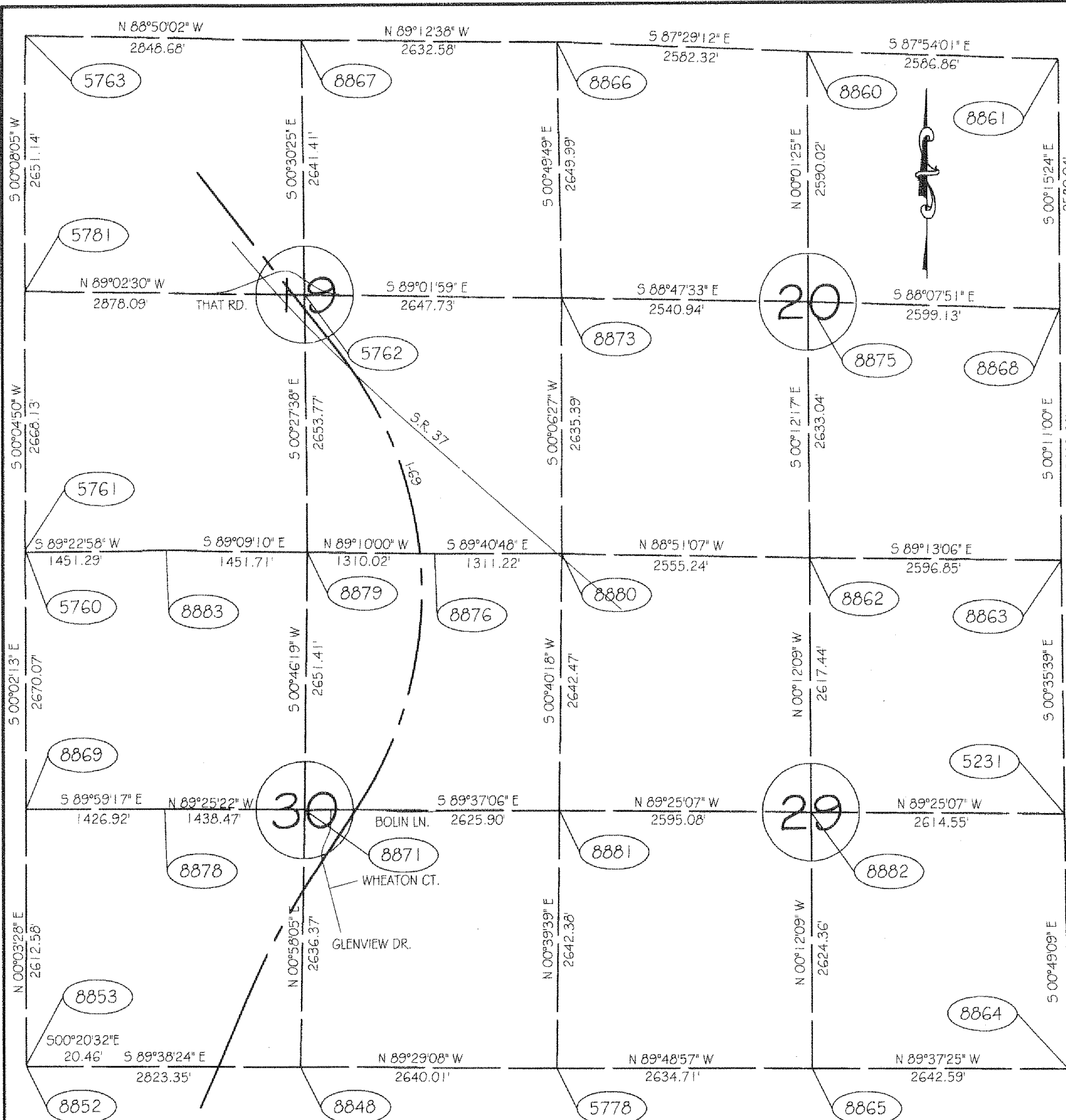
Point #8860 - The northeast corner of the Northwest Quarter of Section 20 - A 5/8” rebar with a plastic cap set by Bledsoe, Tapp & Riggert, Inc. was found 0.7 feet above grade. The Monroe County Surveyors Office Monument Record sheet calls for a “5/8” rebar w/Mundy red cap”. The references from said Monument Record sheet check with the found rebar. There is considered to be no positional uncertainty associated with this corner.

Point #8861 - The northeast corner of the Northeast Quarter of Section 20 - A 3/4” iron pipe was found flush with grade per its' Monroe County Surveyors Office Monument Record sheet. There is considered to be no positional uncertainty associated with this corner.

2011008297 SURVEY2 \$55.00
08/24/2011 03:48:38P 8 PGS
Jim Fielder
Monroe County Recorder IN
Recorded as Presented

JOB NUMBER 101079		<div><div>SURVEYOR'S STATEMENT</div><div><div>THIS SURVEY, TO THE BEST OF MY KNOWLEDGE AND BELIEF, IS EXECUTED ACCORDING TO THE PROVISIONS OF 865 I.A.C. 1-12-20 THROUGH 1-12-25 REGARDING ROUTE SURVEYS EXCEPT THAT ANY DATA SHOWN REGARDING THE LOCATION OR DESCRIPTION OF THE EXISTING PARCELS IS NOT A PART OF THIS SURVEY.</div><div>PREPARED BY: <div><div>BLN</div><div>Beam, Longest and Neff, LLC Consulting Engineers, Land Surveyors, Land Planners & Landscape Architects</div></div><div>8126 Castleton Road, Indianapolis, IN 46250 Phone: 317.849.5832</div><div>www.B-L-N.com Fax: 317.841.4281</div></div></div></div>		PREPARED BY: <i>Dustin K. Ballard</i> REGISTERED LAND SURVEYOR DATE 6/22/11			
SURVEY STARTED MAY 18, 2011				DRAWN: JKR			
SURVEY COMPLETED JUNE 21, 2011		CHECKED: EJS					
ROUTE PLAT SHEETS 5 OF 8		I-69 SECTION 4, SEGMENT 9 EAST ZONE MONROE COUNTY		HORIZONTAL SCALE 1" = 200'		BRIDGE FILE	
				COUNTY MONROE		DESIGNATION NO.	
		LOCATION CONTROL ROUTE SURVEY		SURVEY BOOK		SHEETS	
						of	
				CONTRACT		PROJECT NO.	

SECTION CORNER REFERENCES



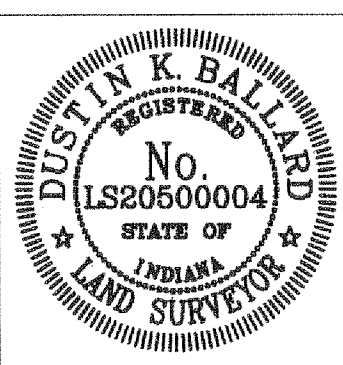
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101079		
SURVEY STARTED		
MAY 18, 2011		
SURVEY COMPLETED		
JUNE 21, 2011		
ROUTE PLAT SHEETS		
6	OF	8

SURVEYOR STATEMENT

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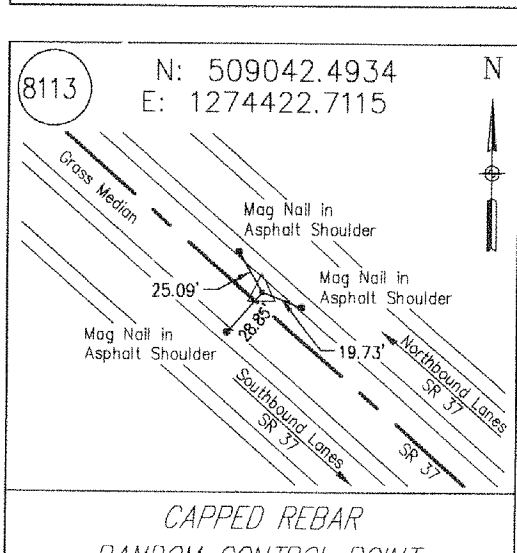
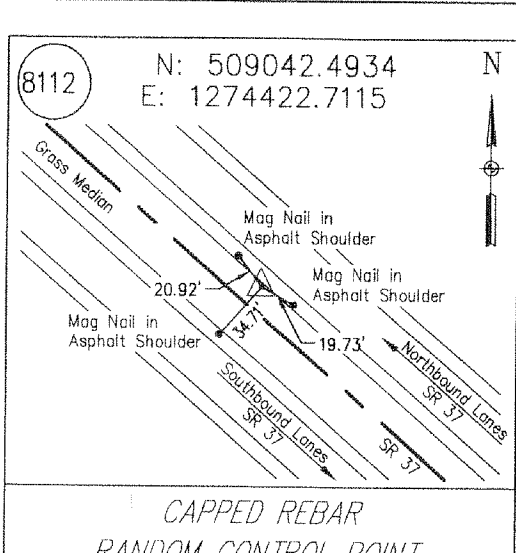
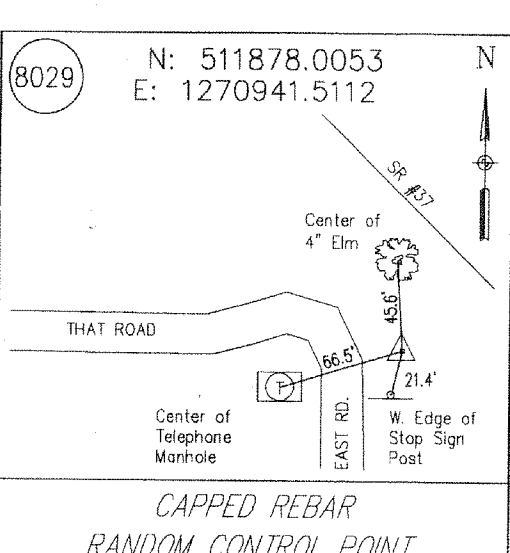
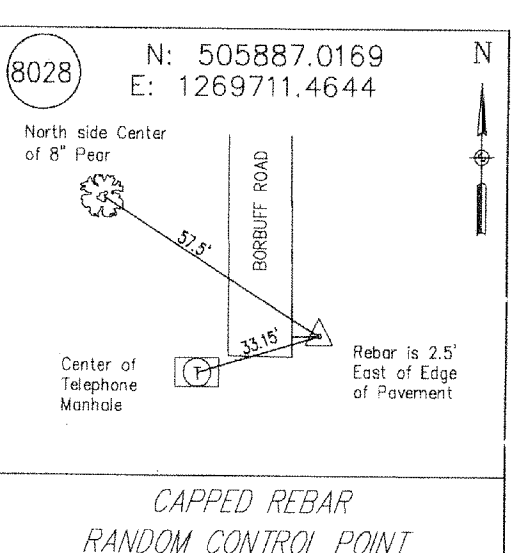
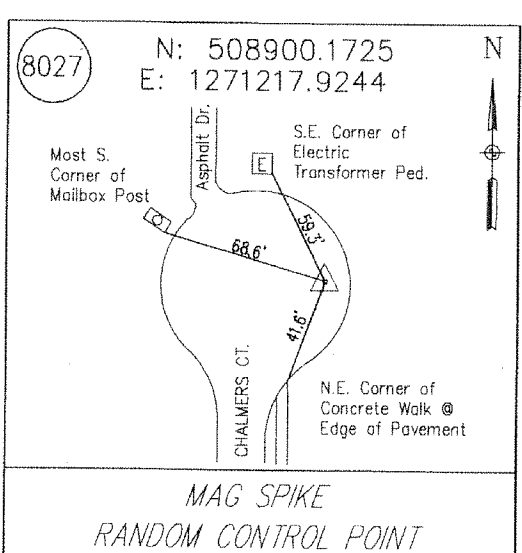
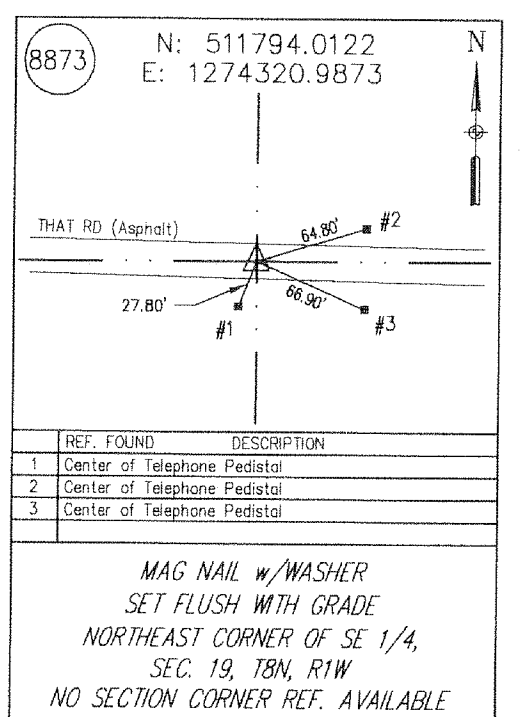
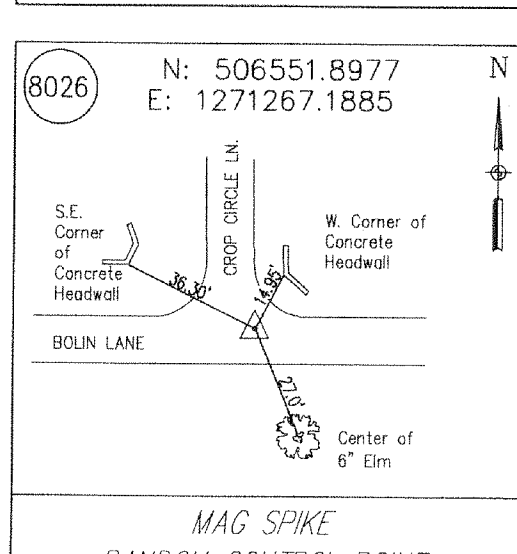
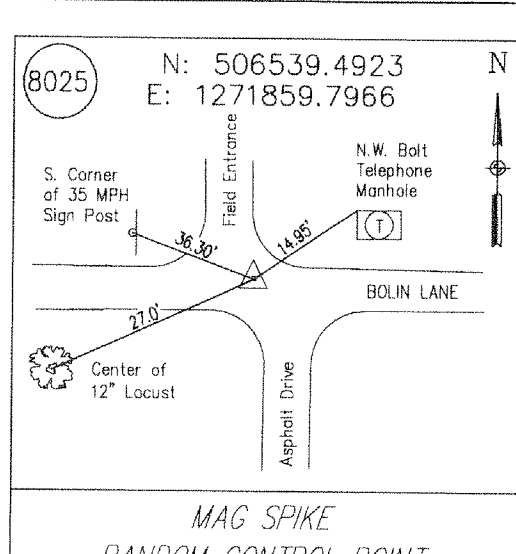
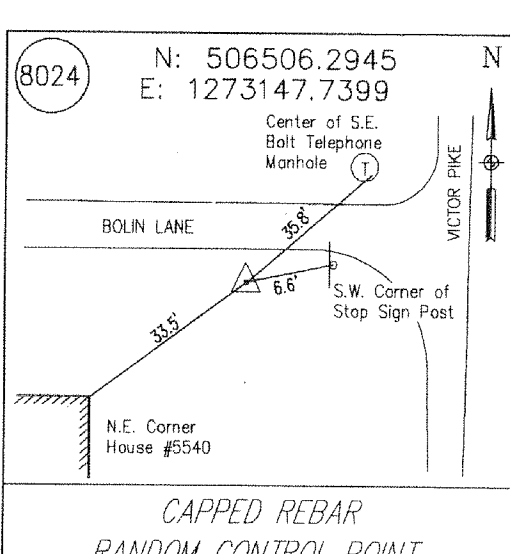
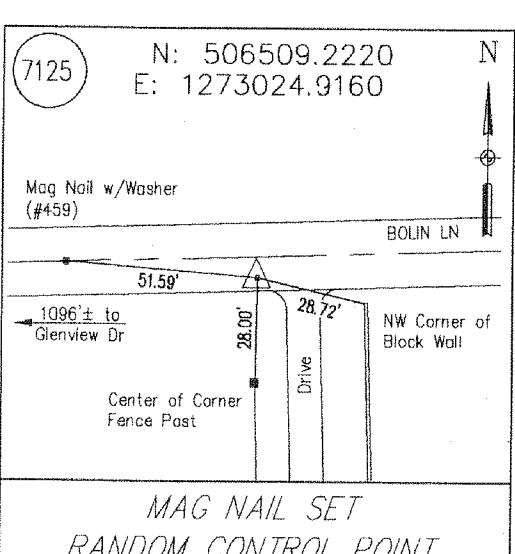
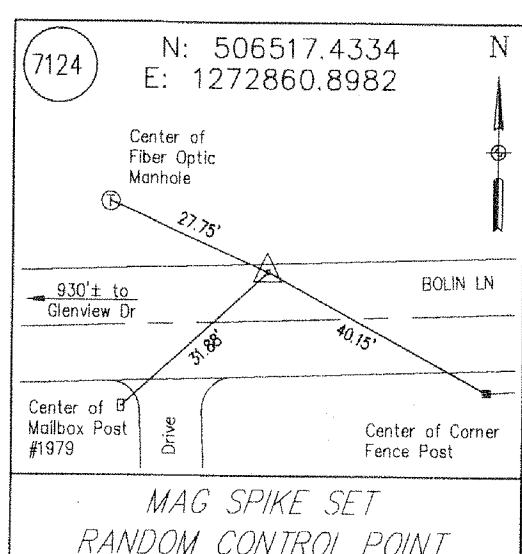
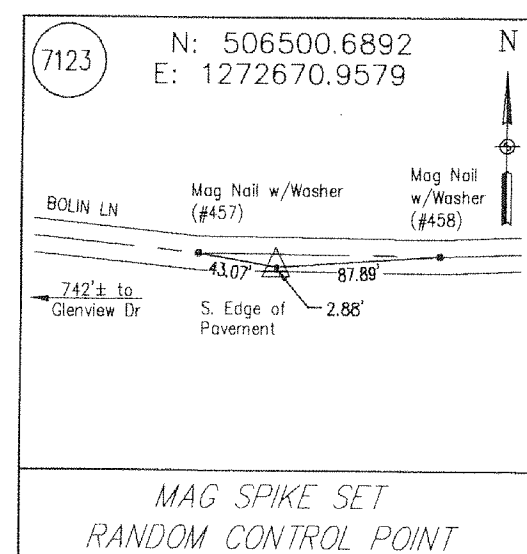
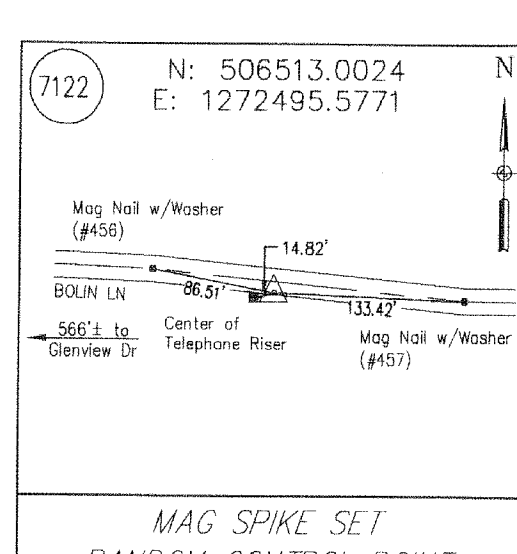
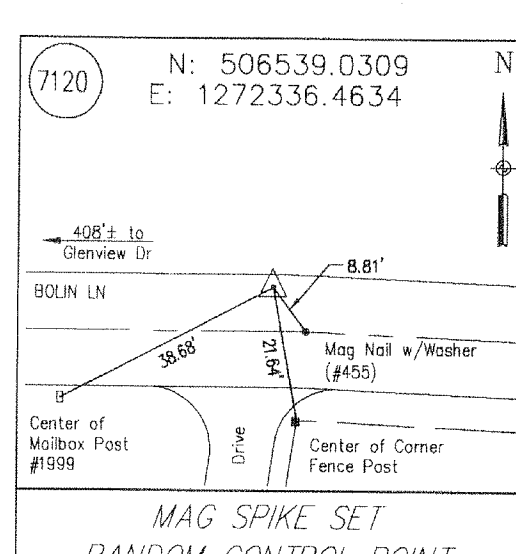
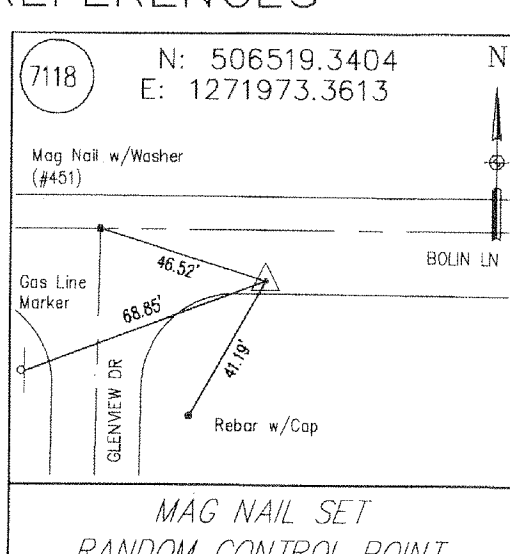
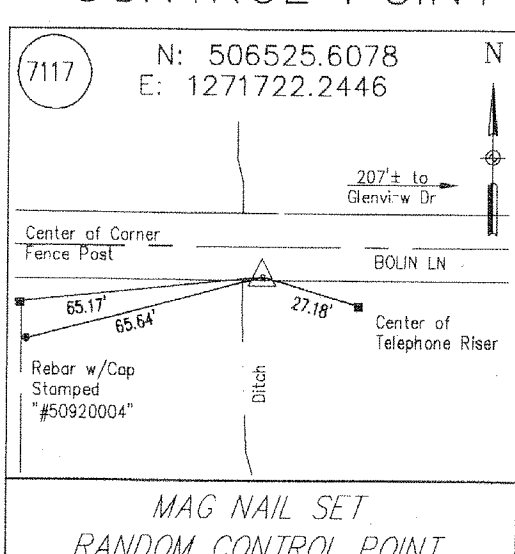
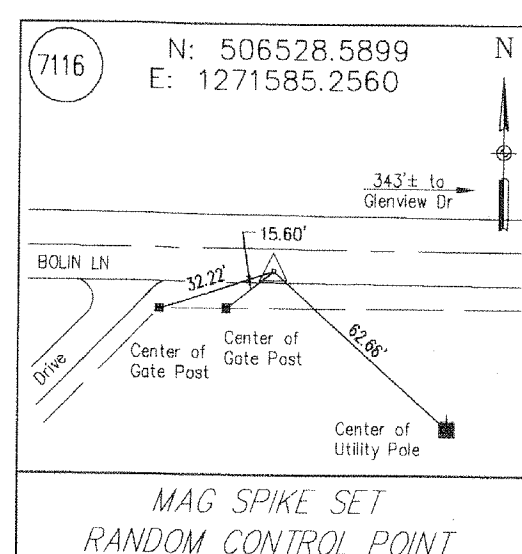
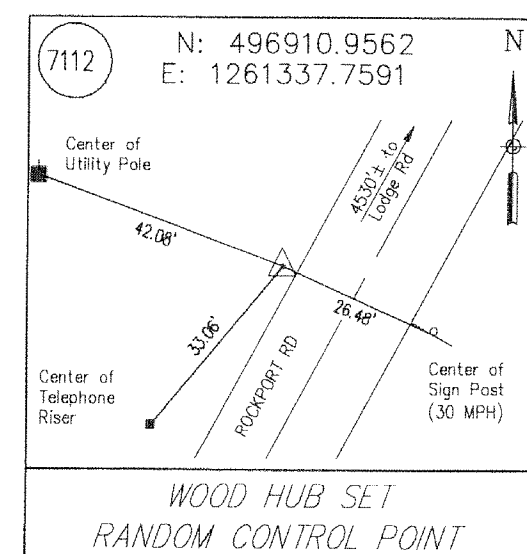
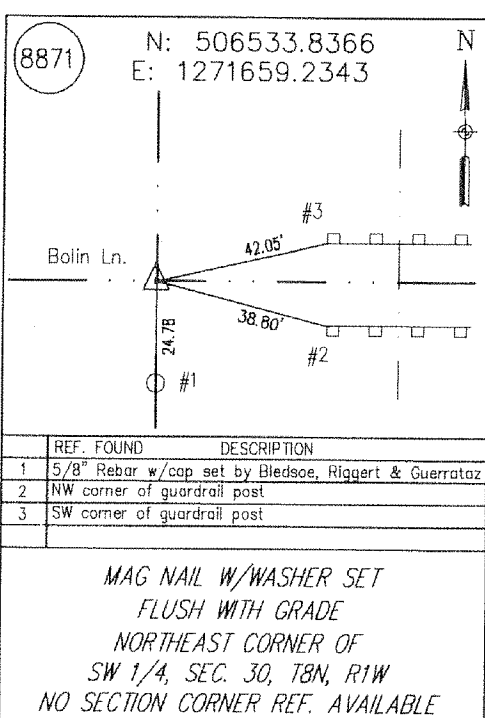
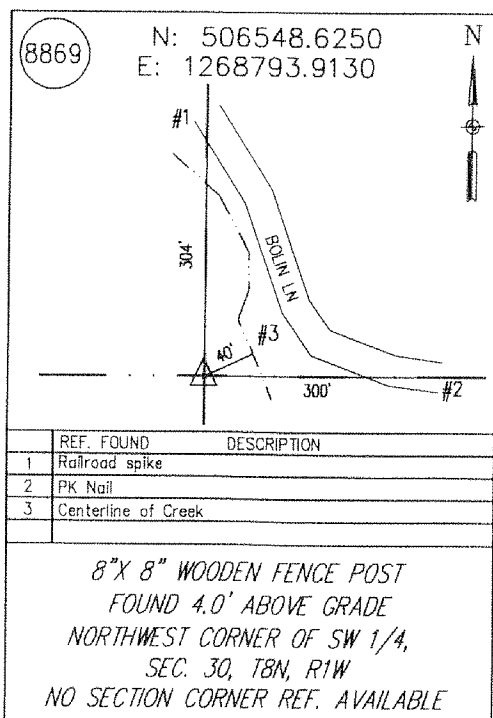
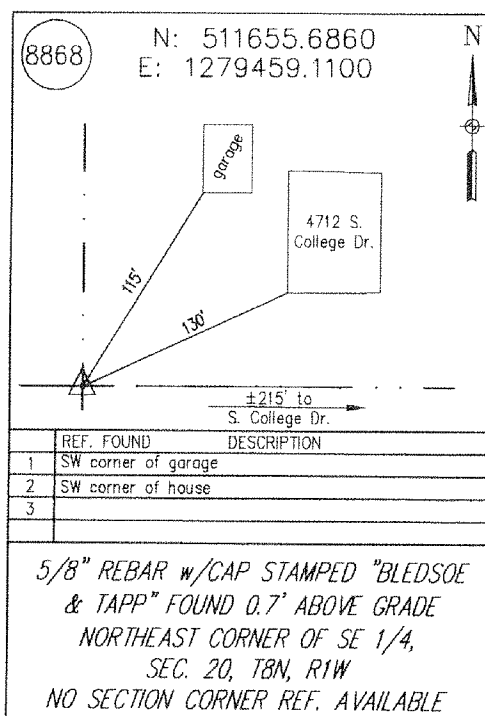
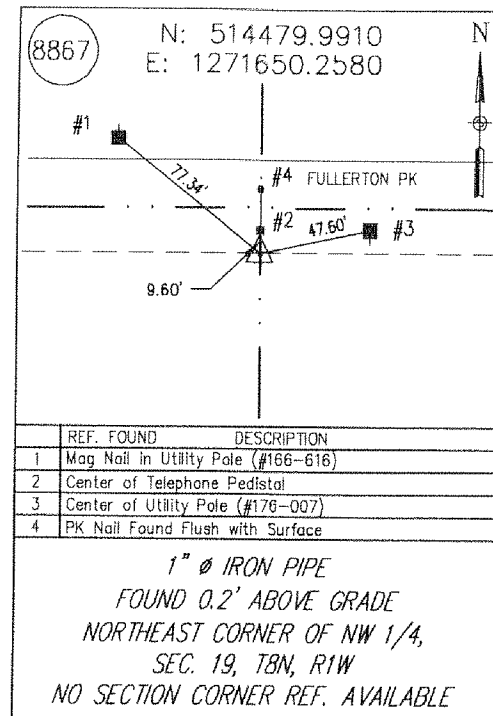
PREPARED BY: <u>Dustin K. Ballard</u>		6/22/11
REGISTERED LAND SURVEYOR		DATE
DRAWN: <u>JKR</u>		
CHECKED: <u>EJS</u>		

**I-69 SECTION 4, SEGMENT 9
EAST ZONE MONROE COUNTY**

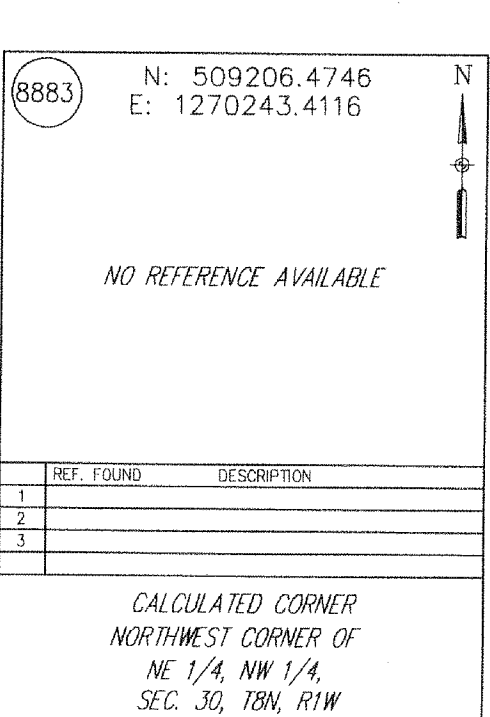
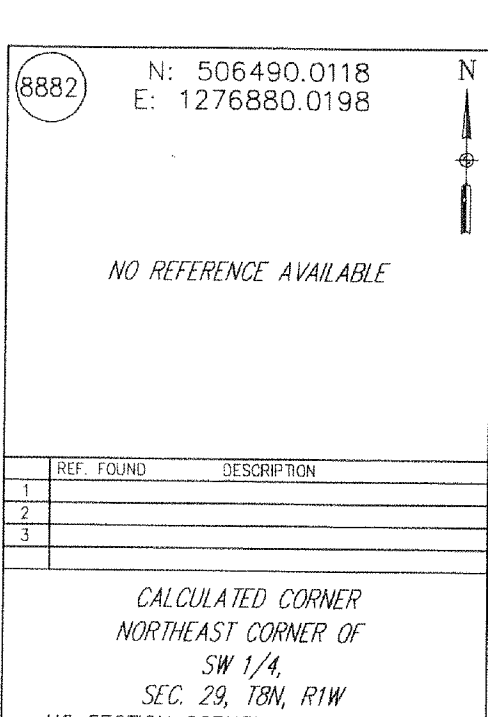
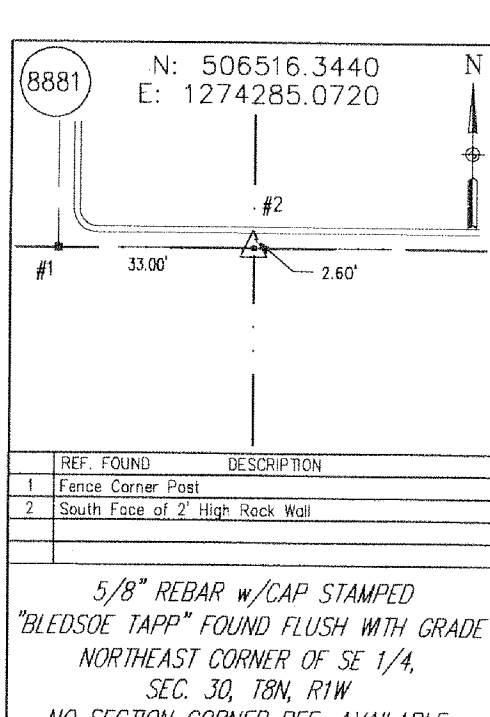
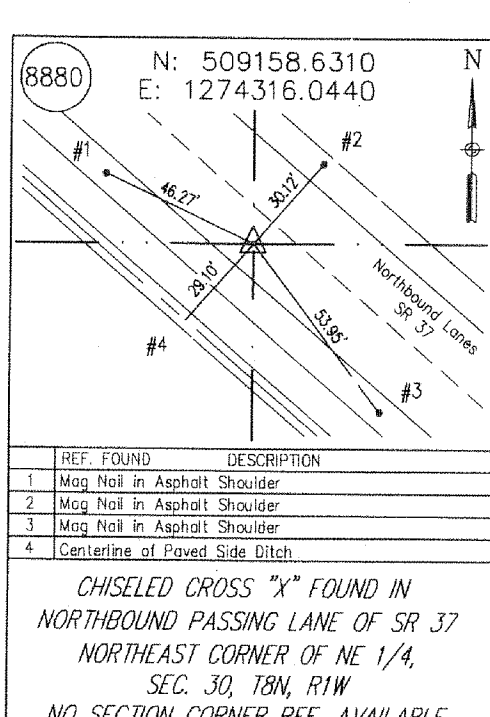
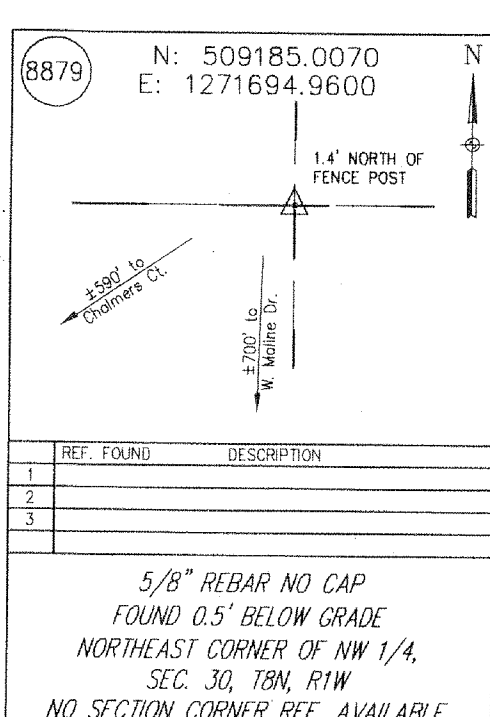
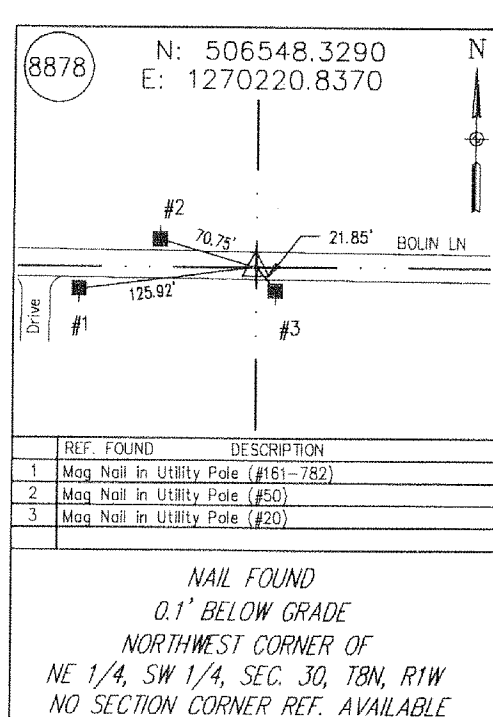
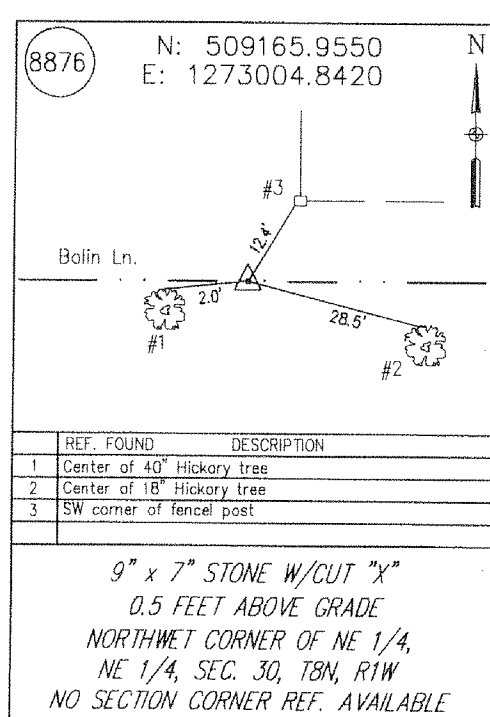
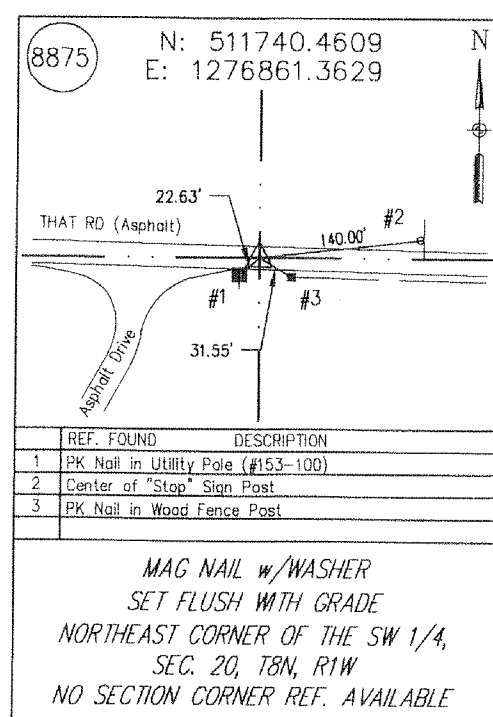
LOCATION CONTROL ROUTE SURVEY

HORIZONTAL SCALE	BRIDGE FILE
1\" = 200'	
COUNTY	DESIGNATION NO.
MONROE	
SURVEY BOOK	SHEETS
	of
CONTRACT	PROJECT NO.

SECTION CORNER REFERENCES (CONTINUED)



SECTION CORNER REFERENCES (CONTINUED)



JOB NUMBER	
101079	
SURVEY STARTED	
APRIL 04, 2011	
SURVEY COMPLETED	
MAY 17, 2011	
ROUTE PLAT SHEETS	
7	OF 8

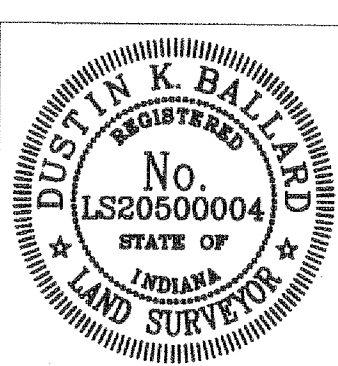
SURVEYOR STATEMENT

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PREPARED BY: <u>Dustin K. Ballard</u>	
REGISTERED LAND SURVEYOR	
DATE: 6/22/11	
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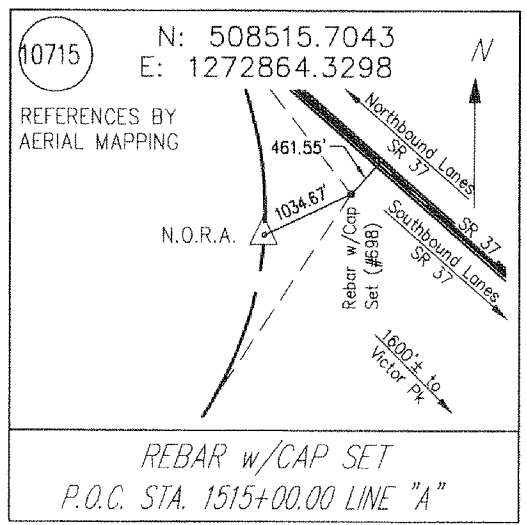
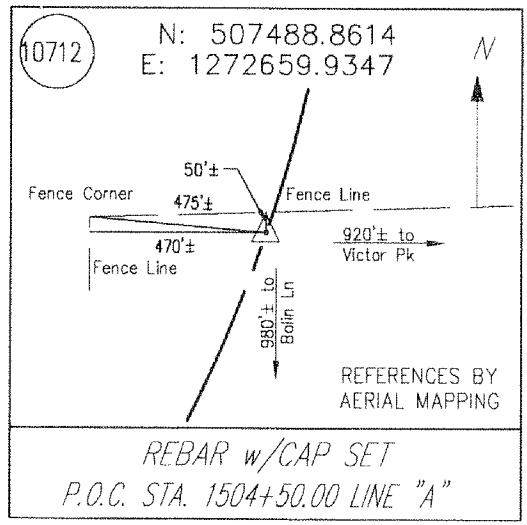
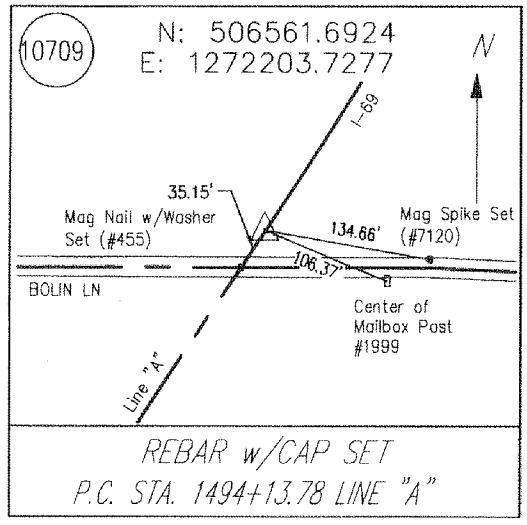
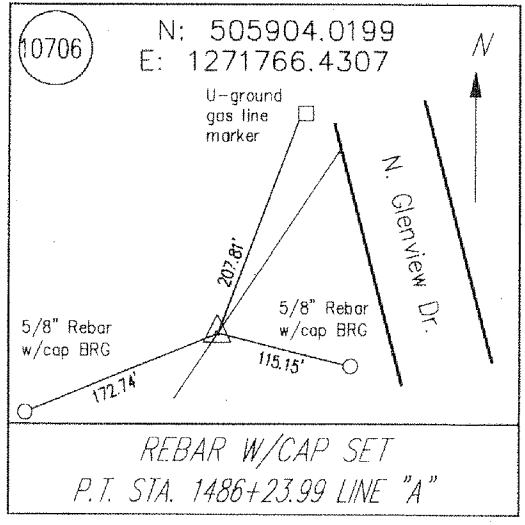
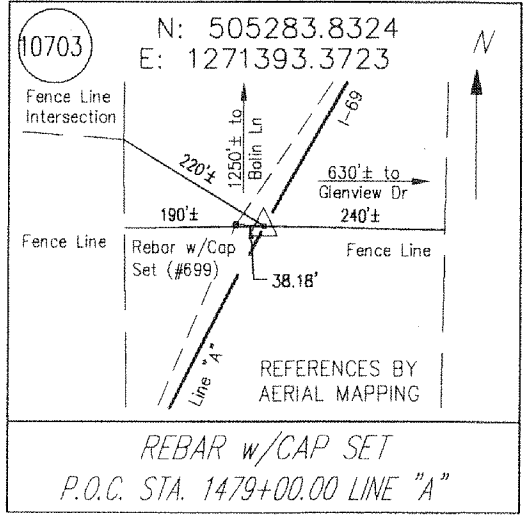
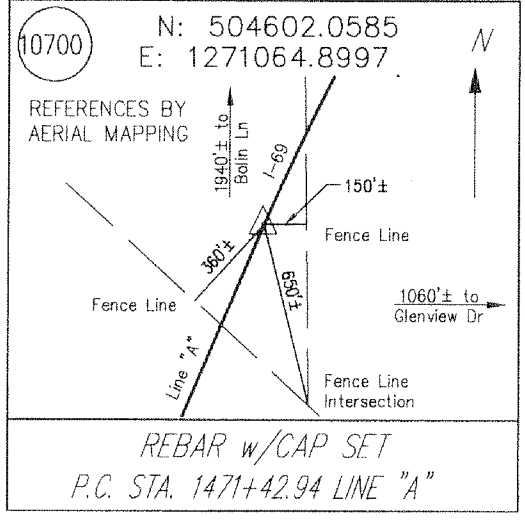
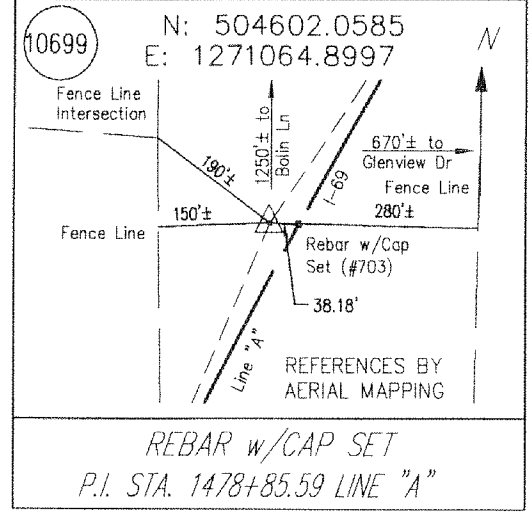
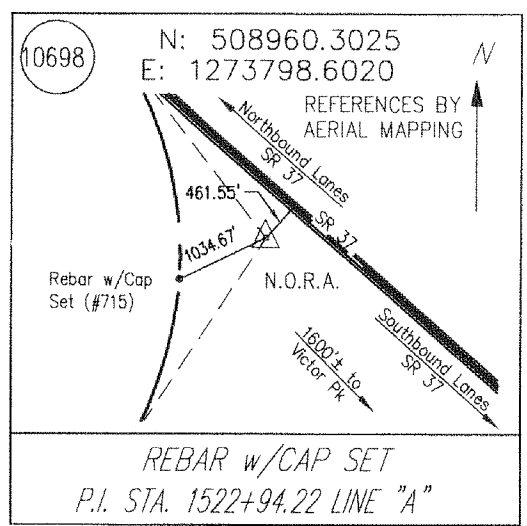
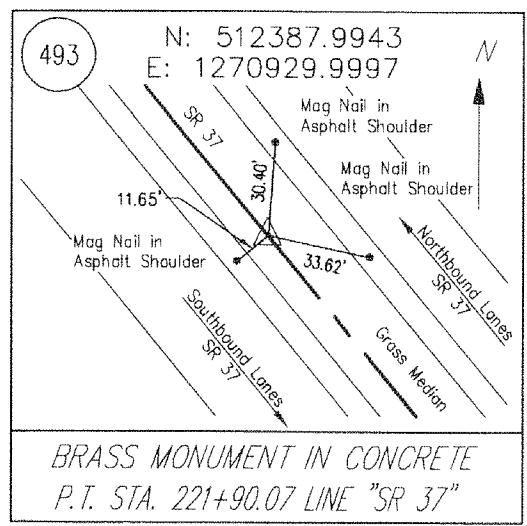
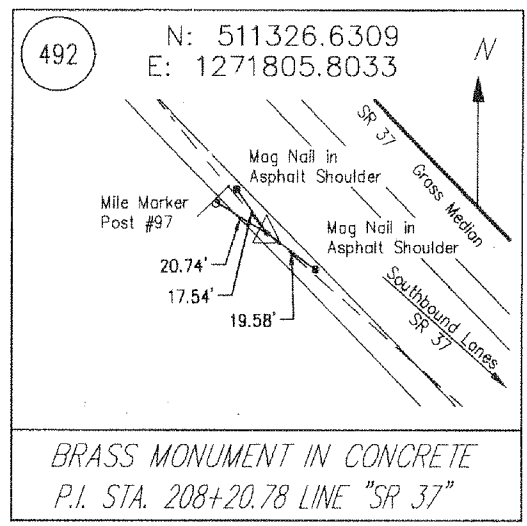
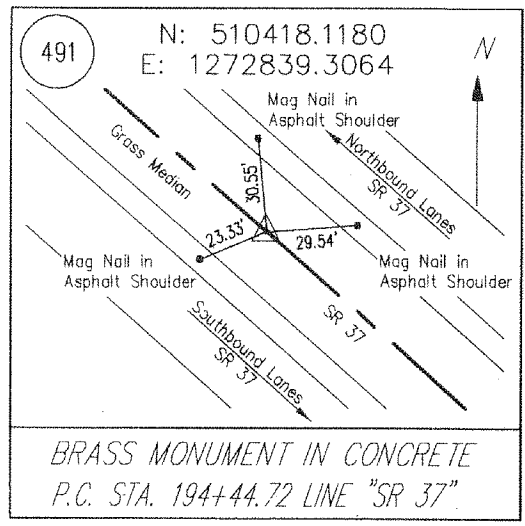
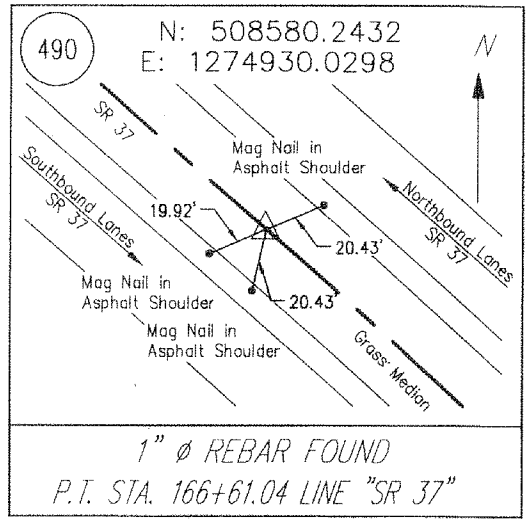
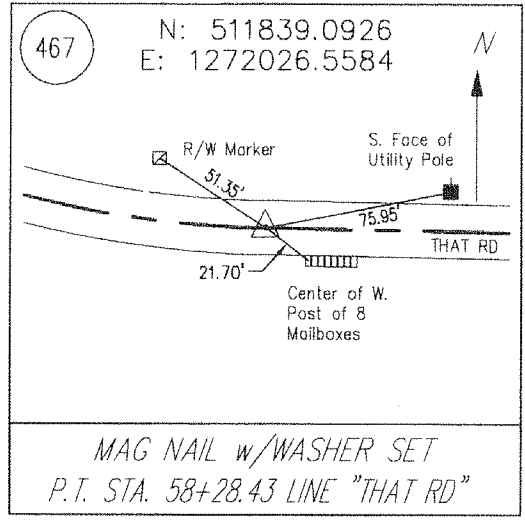
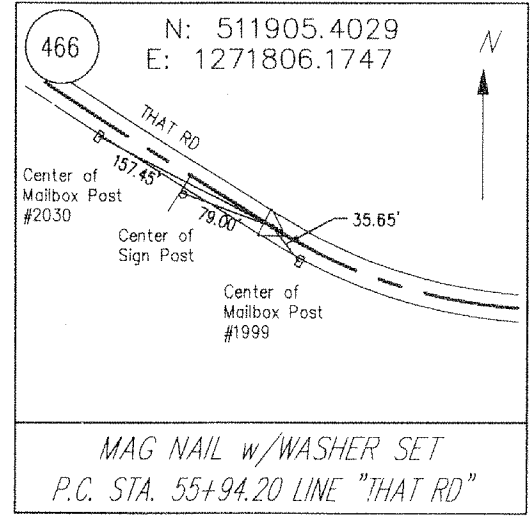
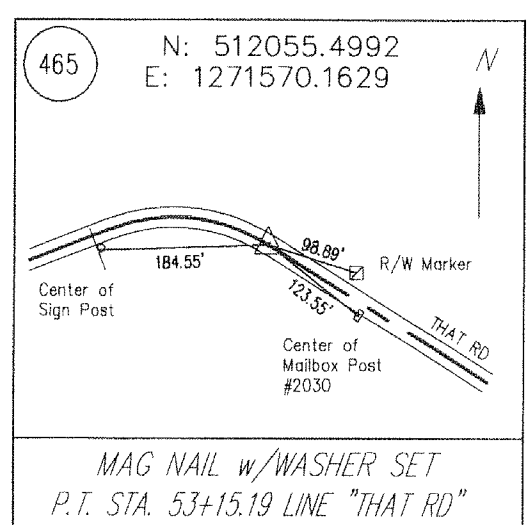
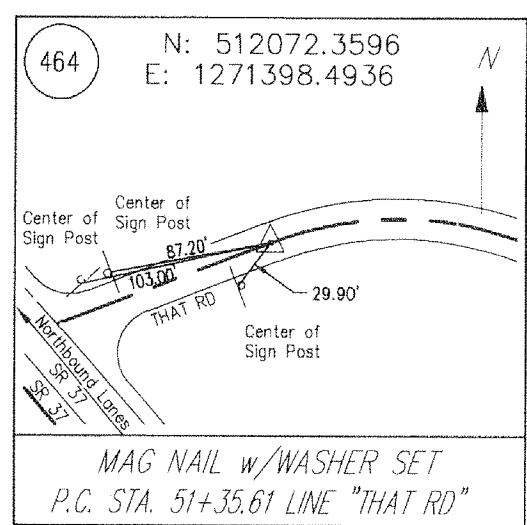
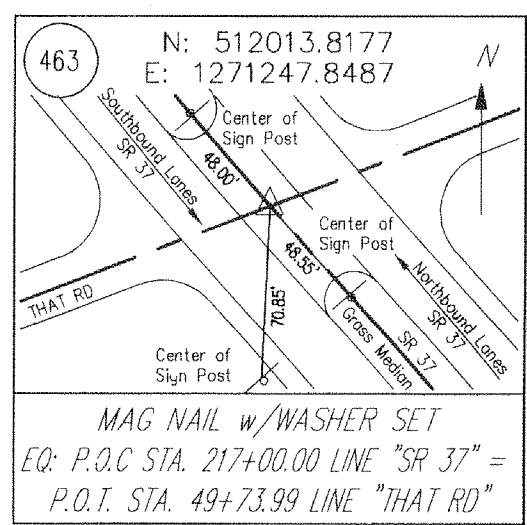
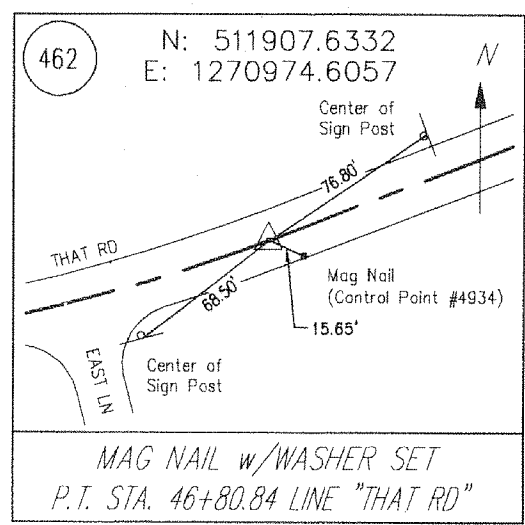
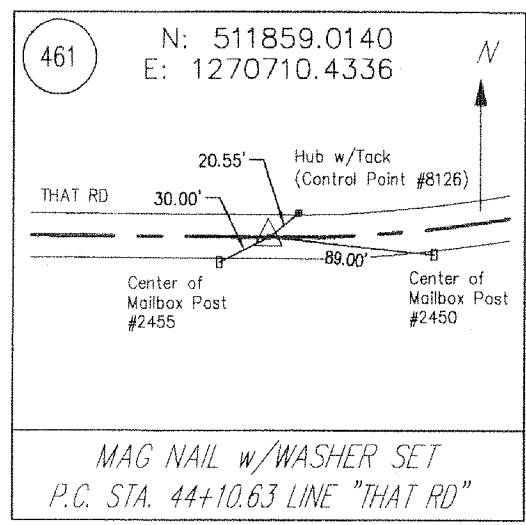
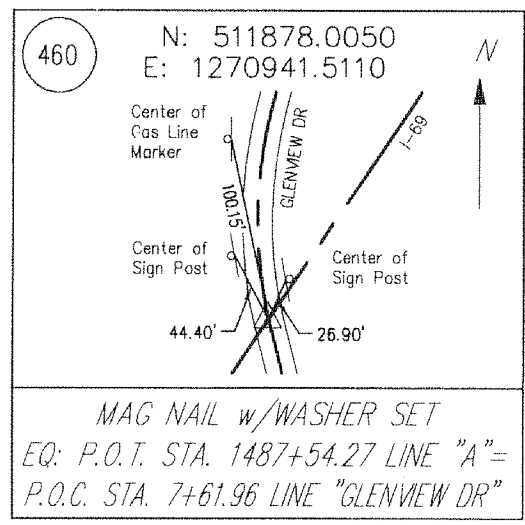
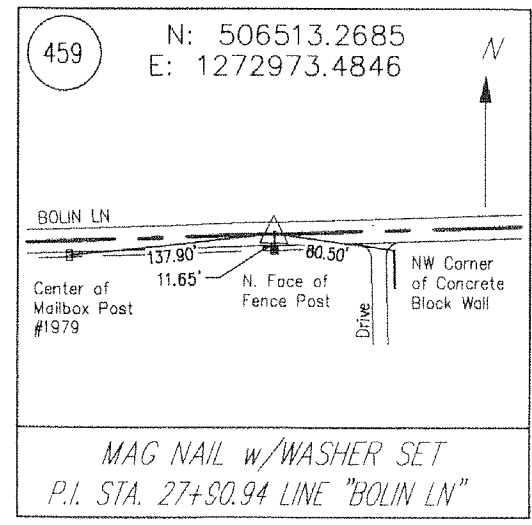
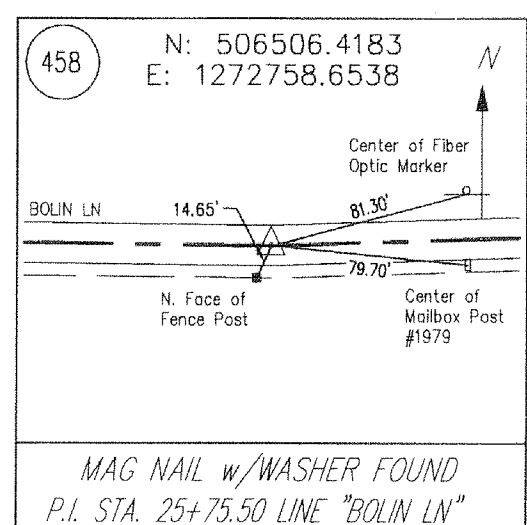
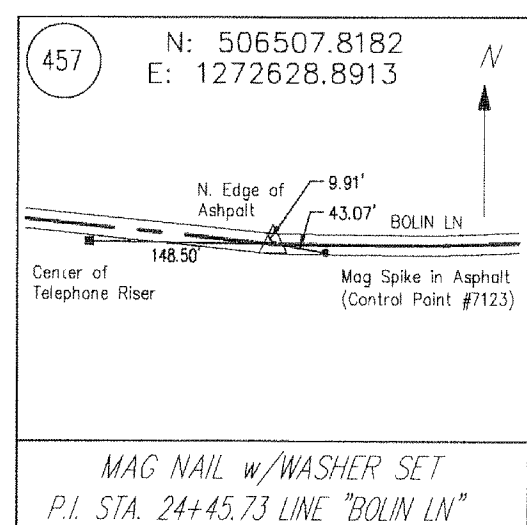
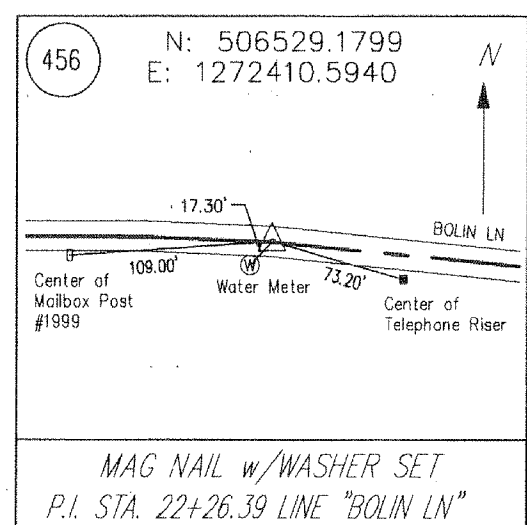
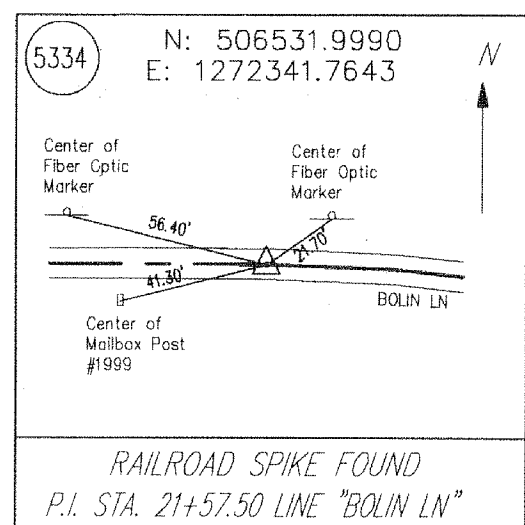
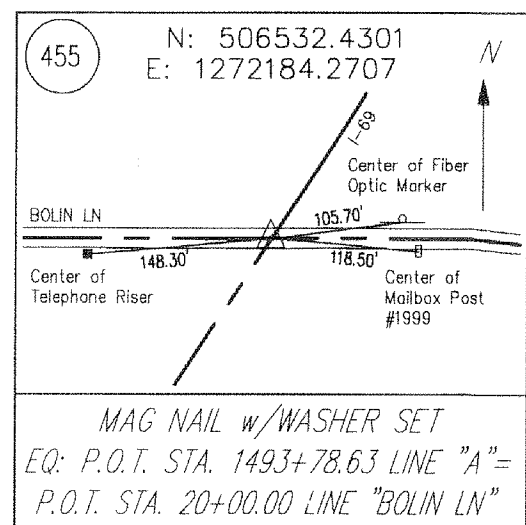
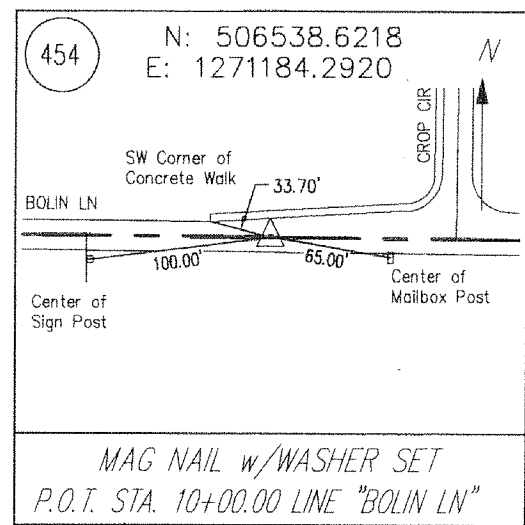
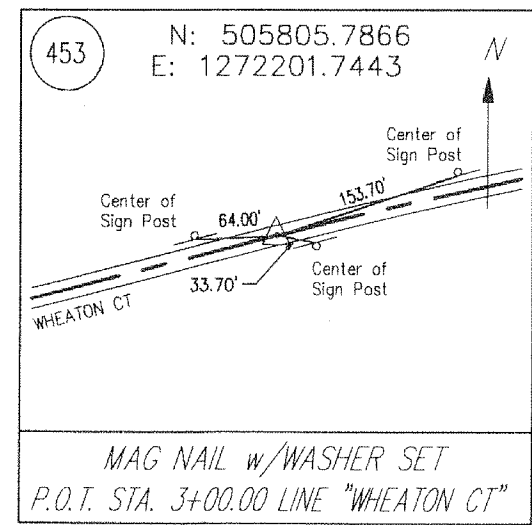
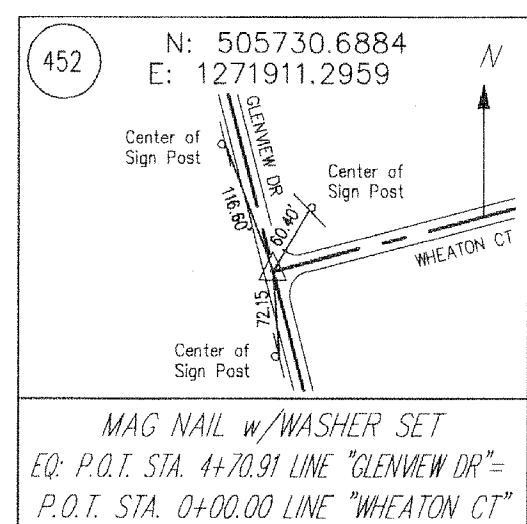
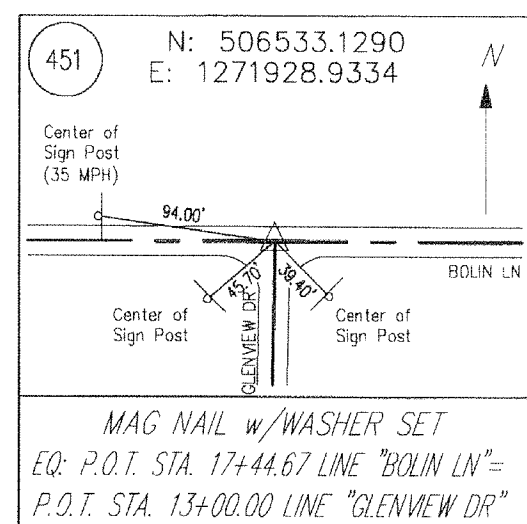
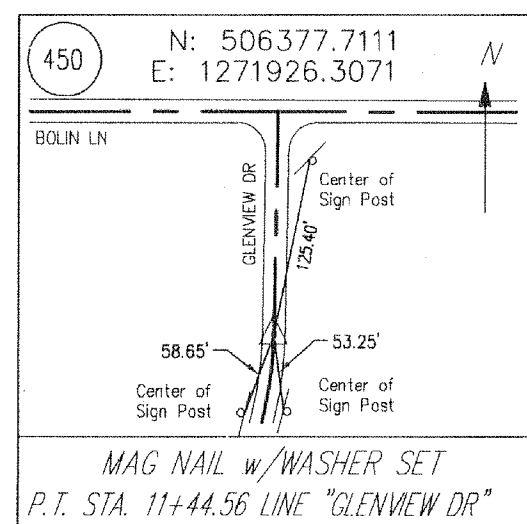
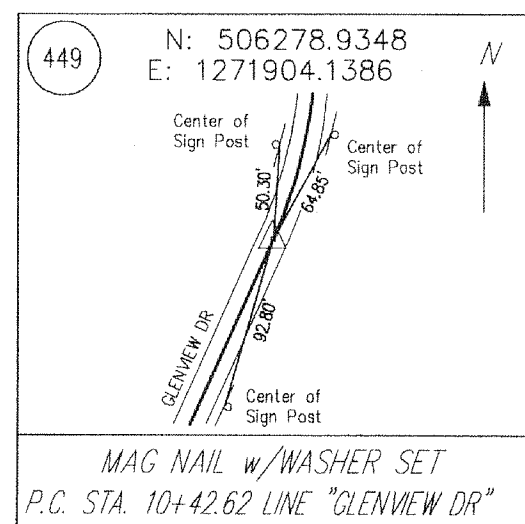
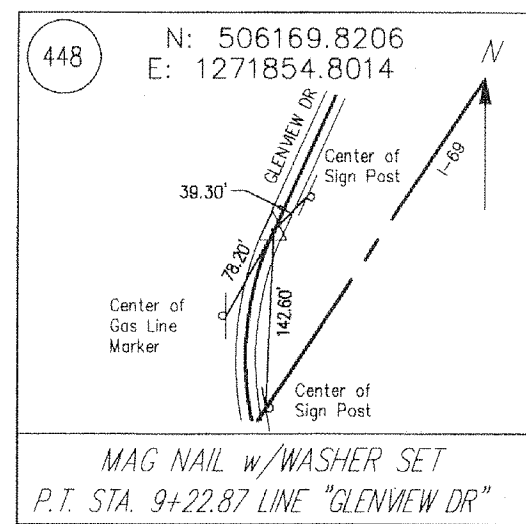
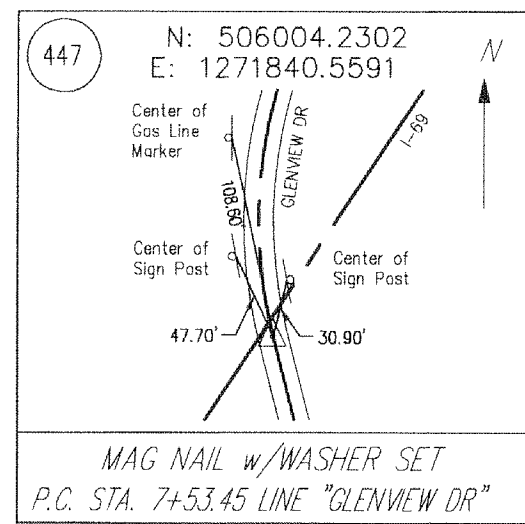
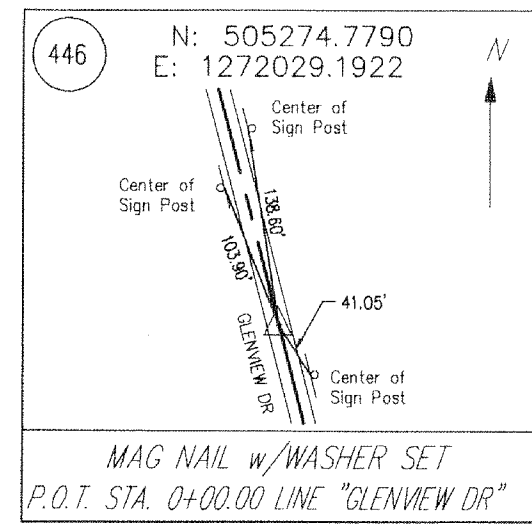
**I-69 SECTION 4, SEGMENT 9
EAST ZONE MONROE COUNTY**

LOCATION CONTROL ROUTE SURVEY

HORIZONTAL SCALE	BRIDGE FILE
1" = 200'	
COUNTY	DESIGNATION NO.
MONROE	
SURVEY BOOK	SHEETS
	of
CONTRACT	PROJECT NO.

ALIGNMENT POINT REFERENCES

2011008297 SURVEY2 \$55.00
06/24/2011 03:48:38P 6 PGS
Jim Fielder
Monroe County Recorder IN
Recorded as Presented



JOB NUMBER	101079
SURVEY STARTED	APRIL 04, 2011
SURVEY COMPLETED	MAY 17, 2011
ROUTE PLAT SHEETS	8 OF 8

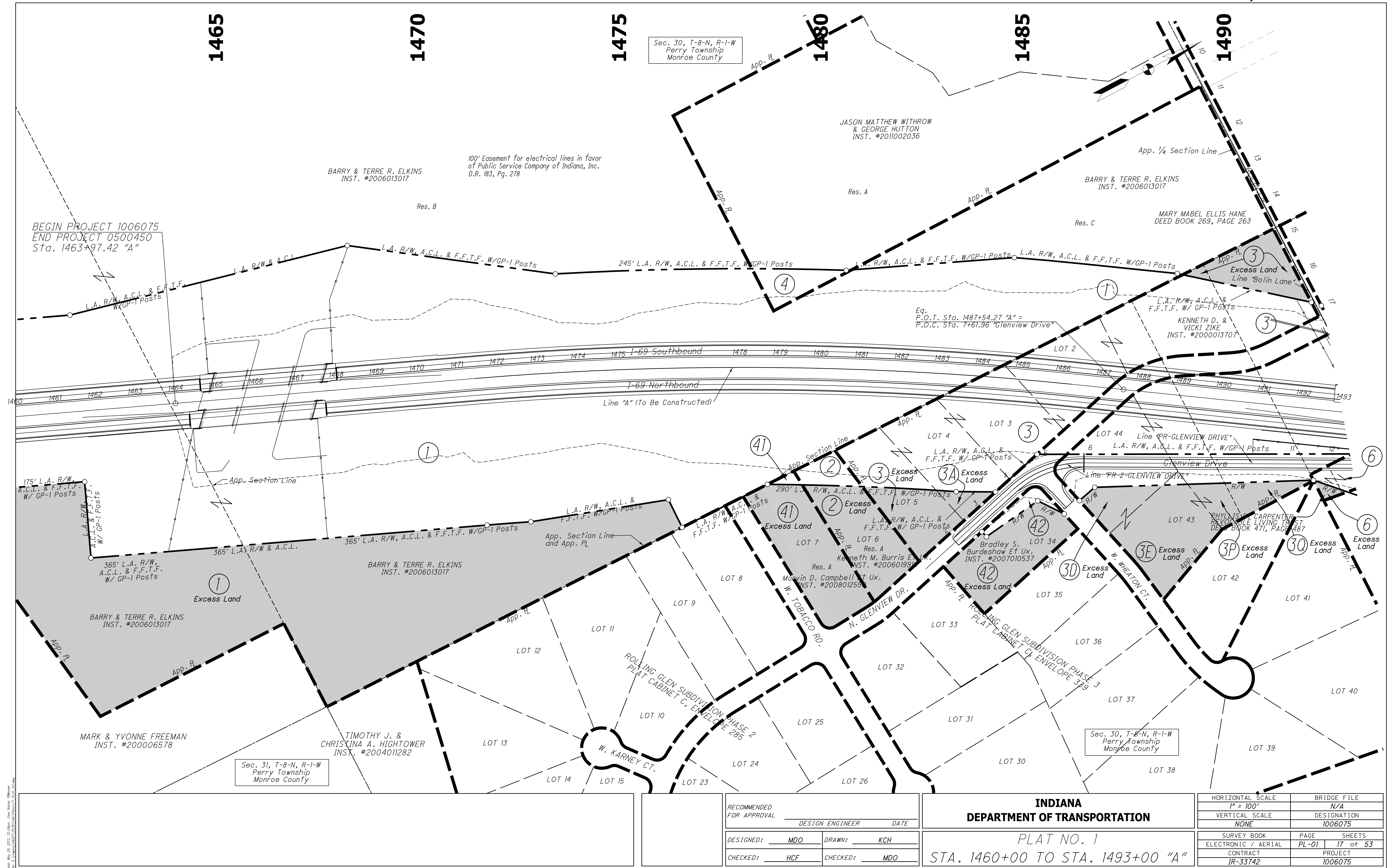
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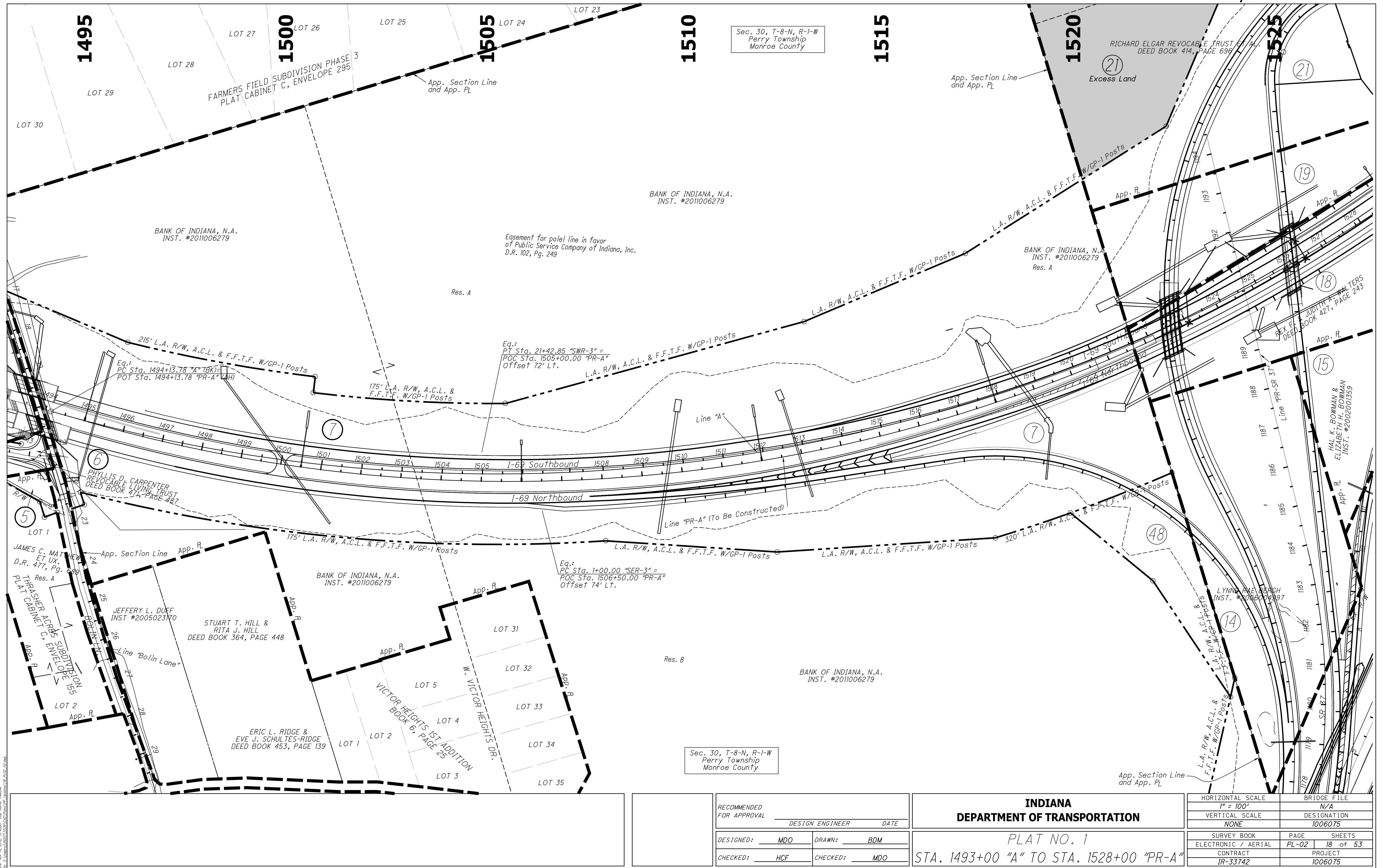


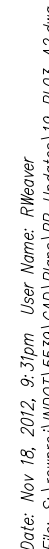
PREPARED BY:	Dustin K. Ballard	6/22/11
REGISTERED LAND SURVEYOR		DATE
DRAWN:	JKR	
CHECKED:	EJS	

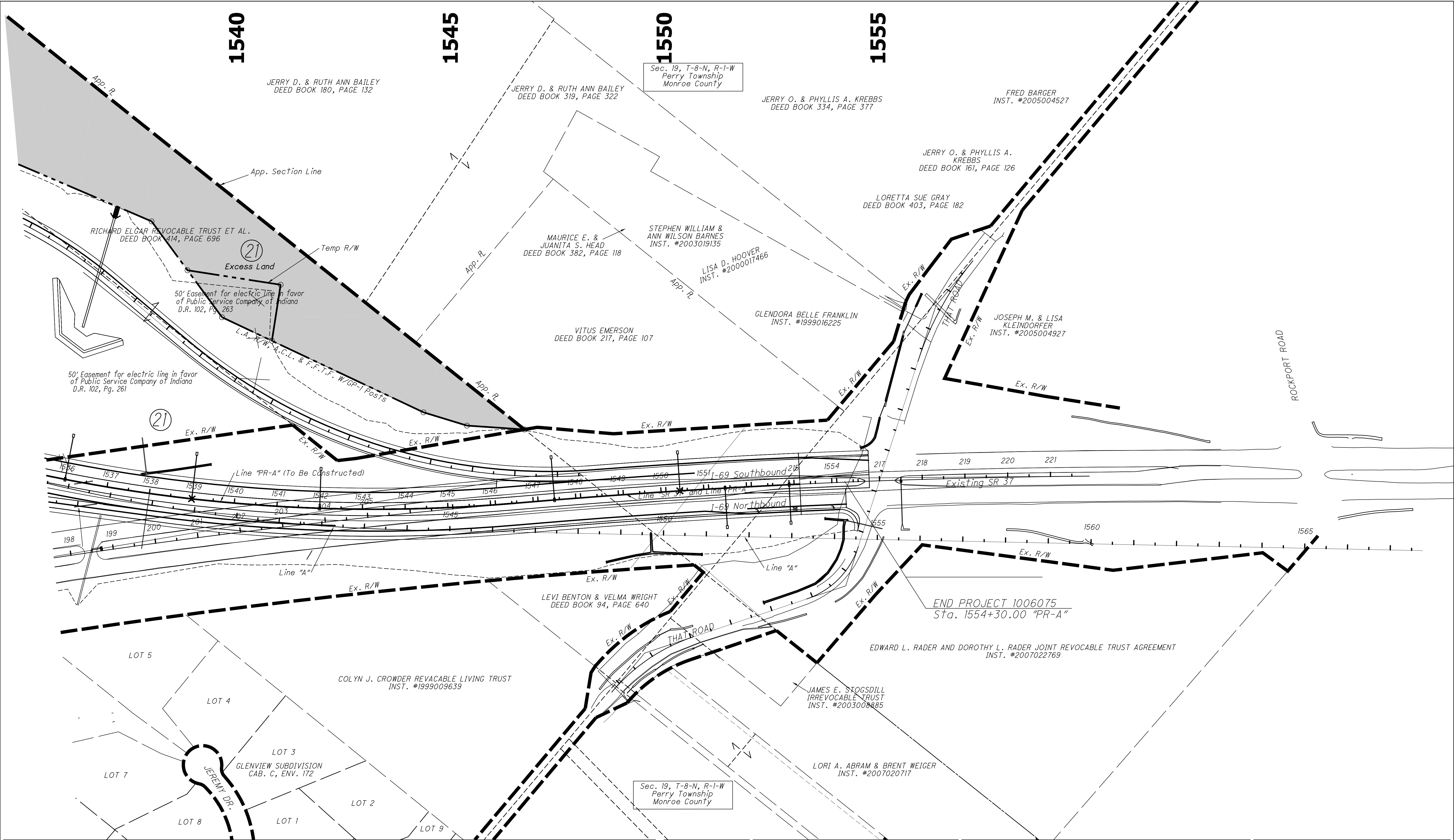
I-69 SECTION 4, SEGMENT 9
EAST ZONE MONROE COUNTY
LOCATION CONTROL ROUTE SURVEY

HORIZONTAL SCALE	BRIDGE FILE
1" = 200'	
COUNTY	DESIGNATION NO.
MONROE	
SURVEY BOOK	SHEETS
CONTRACT	of
	PROJECT NO.

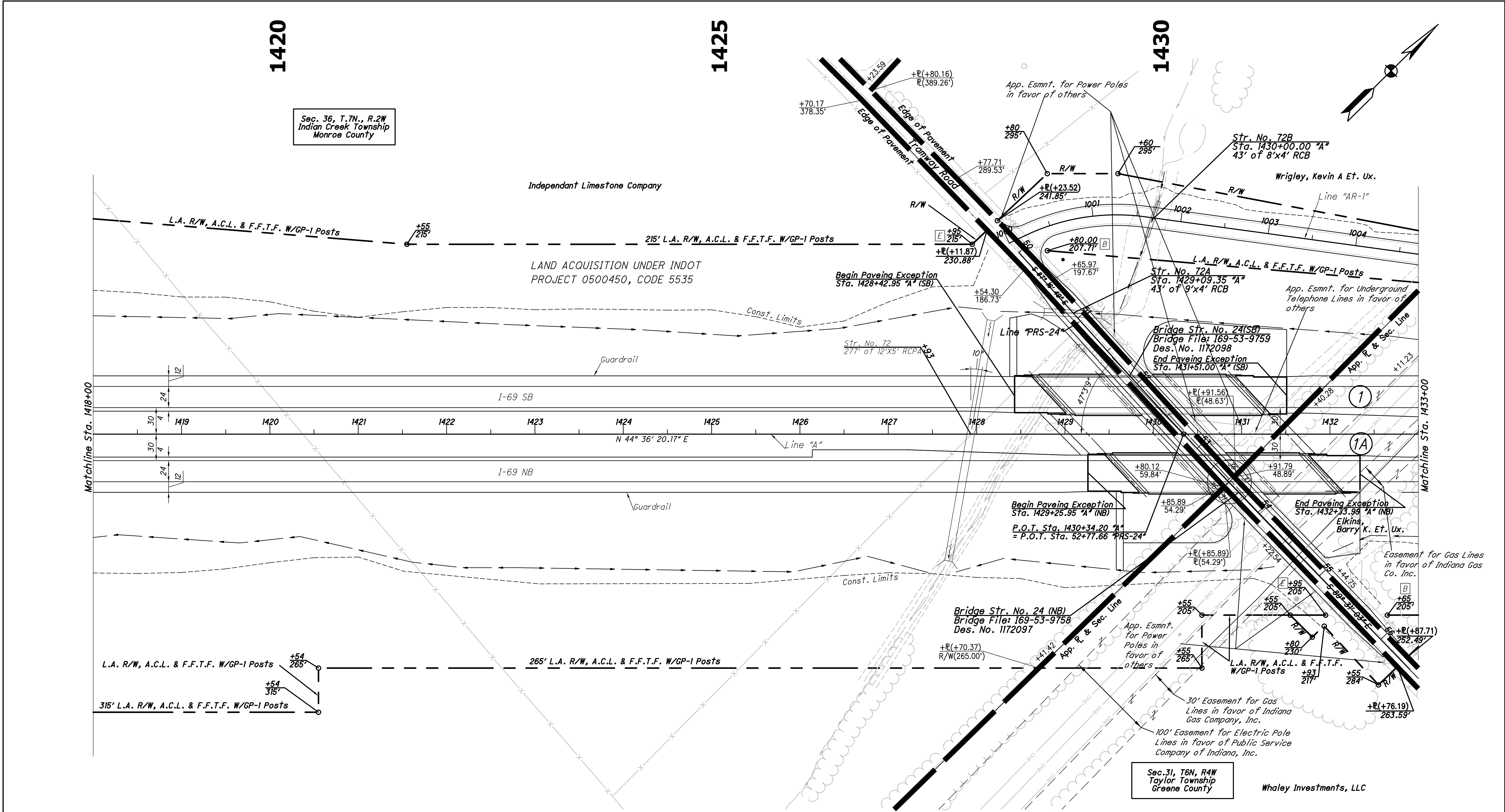








<p>RECOMMENDED FOR APPROVAL</p> <p>DESIGNED: <u>MDO</u> DRAWN: <u>BDM</u></p> <p>CHECKED: <u>HCF</u> CHECKED: <u>MDO</u></p>		<p>INDIANA DEPARTMENT OF TRANSPORTATION</p>		<p>HORIZONTAL SCALE 1" = 100'</p>		<p>BRIDGE FILE N/A</p>	
		<p>PLAT NO. 1</p> <p>STA. 1535+00 TO STA. 1554+87.53 "PR-A"</p>		<p>VERTICAL SCALE NONE</p>		<p>DESIGNATION 1006075</p>	
				<p>SURVEY BOOK ELECTRONIC / AERIAL</p>		<p>PAGE PL-04</p>	
				<p>CONTRACT IR-33742</p>		<p>SHEETS 20 of 53</p>	
						<p>PROJECT 1006075</p>	



Note:
All R/W On This Sheet To Be As Shown
All R/W On This Sheet Described From Line "A" Unless Otherwise Noted
L.A. R/W To Apply Where Indicated

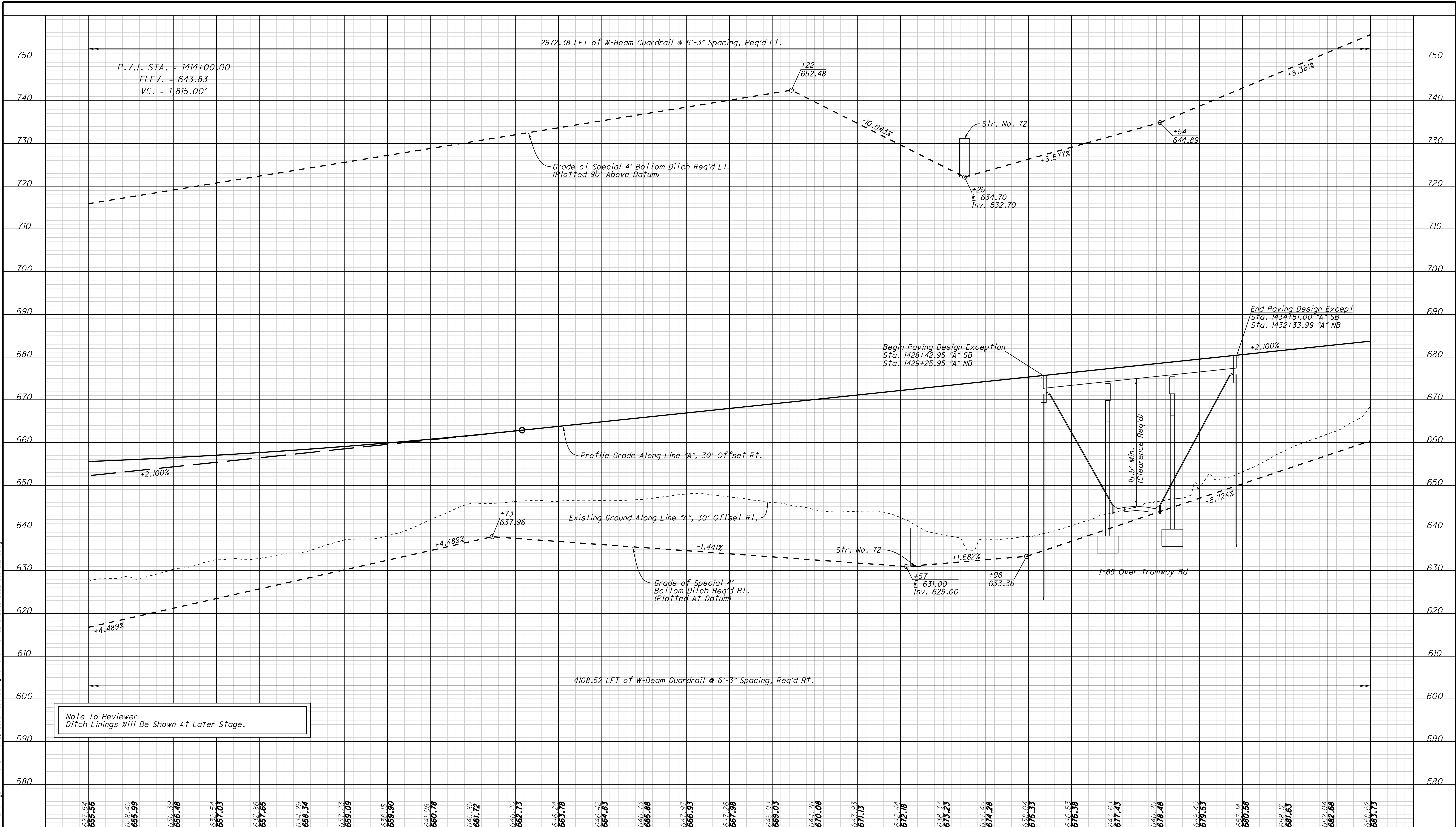
Legend	
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E	Ending L.A. R/W

RECOMMENDED FOR APPROVAL	
DESIGNED:	CHK
CHECKED:	AJD
DRAWN:	ETD
CHECKED:	JAC

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN SHEET	
STA. 1418+00 TO STA. 1433+00 "A"	

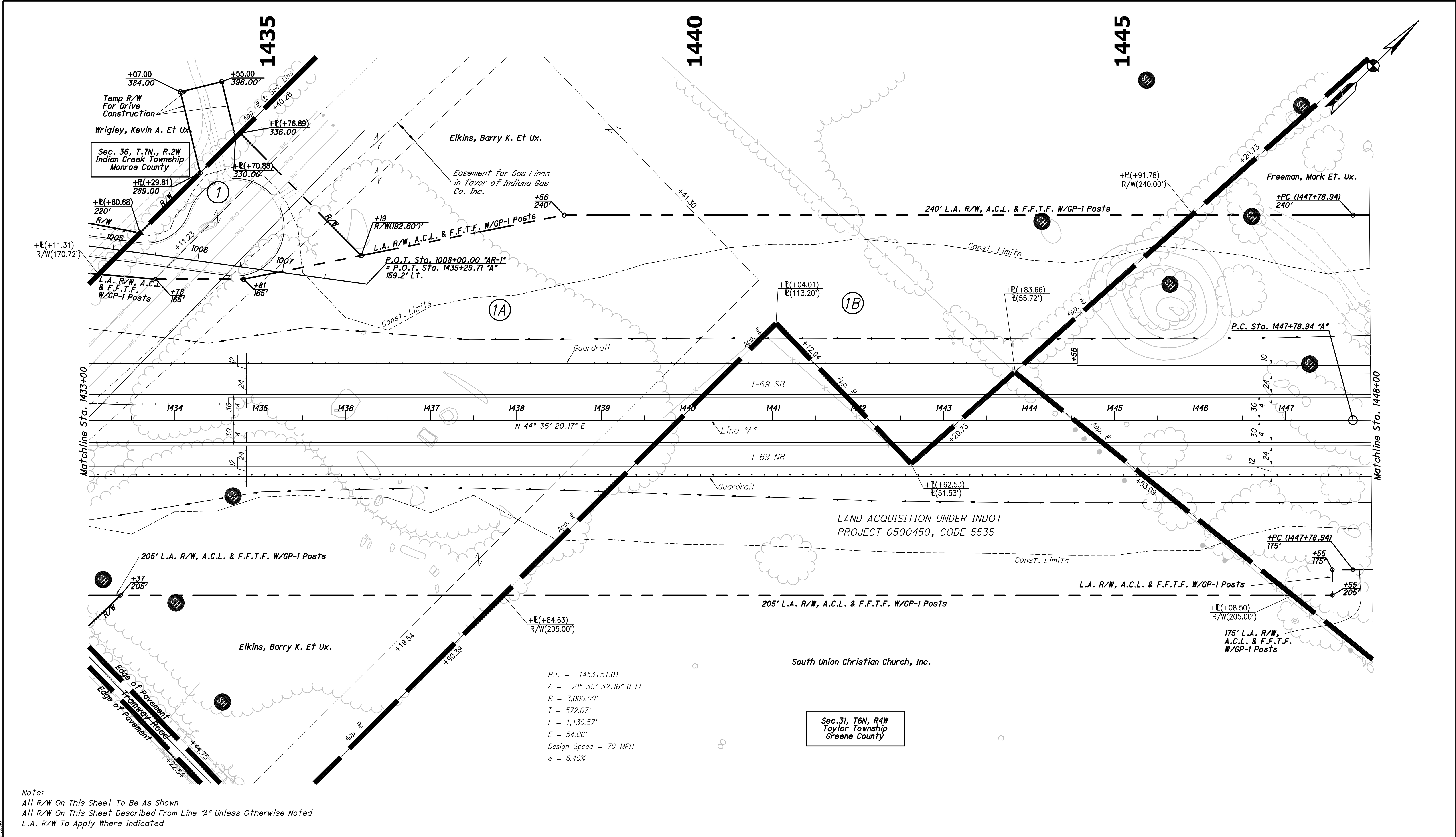
HORIZONTAL SCALE		BRIDGE FILE	
1" = 50'			
VERTICAL SCALE		DESIGNATION	
N/A		1006075	
SURVEY BOOK		PAGE	
ELECTRONIC / AERIAL		21 of 53	
CONTRACT		PROJECT	
1P-33742		1006075	

DATE: 11/16/2011
TIME: 9:28:30 AM
LOCATION: N:\Projects\25627500\Drawings\4 Transp\Road\SB\Y_Plan and Profiles\25627500\SP2-SB.dgn



Note To Reviewer
Ditch Linings Will Be Shown At Later Stage.

1418+00	1419+00	1420+00	1421+00	1422+00	1423+00	1424+00	1425+00	1426+00	1427+00	1428+00	1429+00	1430+00	1431+00	1432+00	1433+00
							<div>RECOMMENDED FOR APPROVAL</div> <div>DESIGN ENGINEER</div> <div>DATE</div>	<div>INDIANA DEPARTMENT OF TRANSPORTATION</div>					<div>HORIZONTAL SCALE</div> <div>1"=50'</div>	<div>BRIDGE FILE</div>	
								<div>PROFILE SHEET</div>					<div>VERTICAL SCALE</div> <div>1"=10'</div>	<div>DESIGNATION</div> <div>0500450</div>	
								<div>STA. 1418+00 to STA. 1433+00 "A"</div>					<div>SURVEY BOOK</div> <div>ELECTRONIC / AERIAL</div>	<div>PAGE</div> <div>PS12</div>	<div>SHEETS</div> <div>46 of 66</div>
													<div>CONTRACT</div> <div>IR-33741</div>	<div>PROJECT</div> <div>0500450</div>	



Notes:
All R/W On This Sheet To Be As Shown
All R/W On This Sheet Described From Line "A" Unless Otherwise Noted
L.A. R/W To Apply Where Indicated

Legend

SH Sinkhole

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: DCK	DRAWN: ETD	
CHECKED: AJD	CHECKED: JAC	

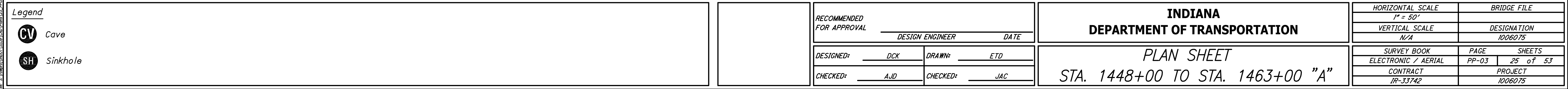
INDIANA DEPARTMENT OF TRANSPORTATION

PLAN SHEET

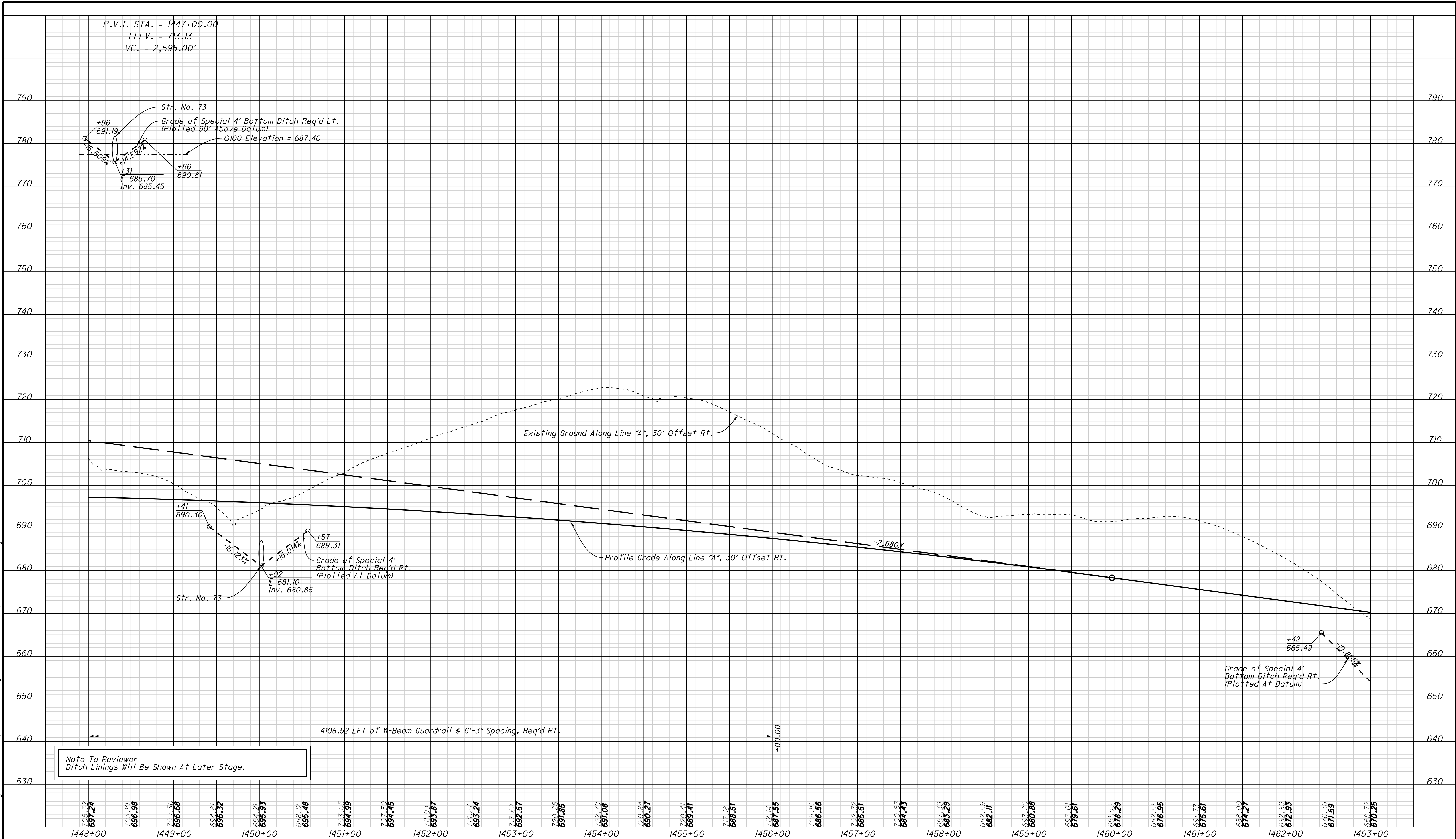
STA. 1433+00 TO STA. 1448+00 "A"

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 1006075
SURVEY BOOK ELECTRONIC / AERIAL	PAGE PP-02
CONTRACT 1P-33742	SHEETS 23 of 53
	PROJECT 1006075

Drawn: 10/26/2017 10:23:11 AM
Sheet: 23 of 53
Project: 1006075
Contract: 1P-33742
Scale: 1" = 50'



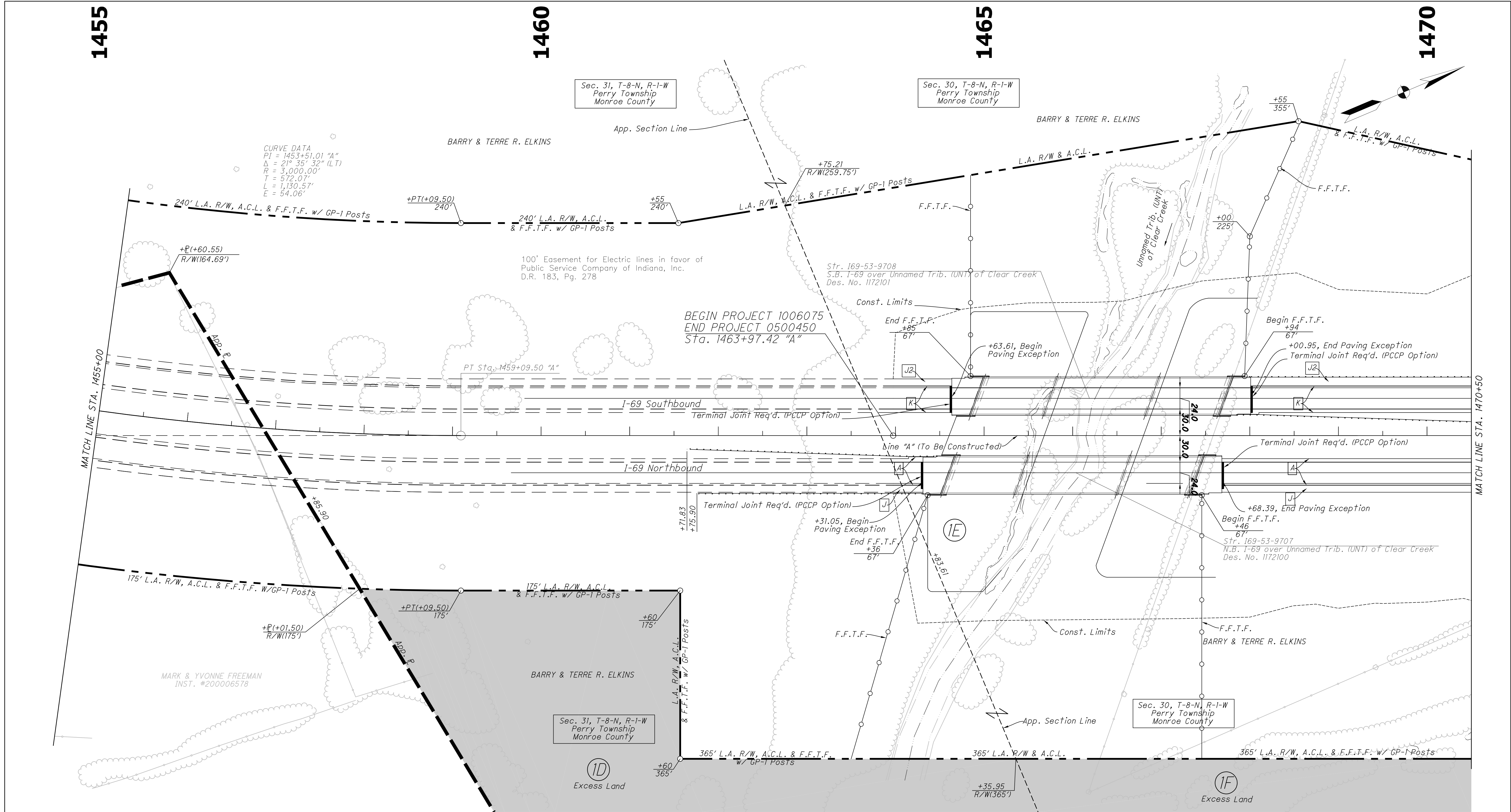
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TIME: 9:28:36 AM
LOCATION: N:\Projects\25627500\Drawings\4 Trans\Transp\Cadd\Road\SB\Y_Plan and Profiles\Sheets\25627500\PS14-SB.dgn



RECOMMENDED FOR APPROVAL	
DESIGNED: DCK	DRAWN: ETD
CHECKED: AJD	CHECKED: JAC

INDIANA DEPARTMENT OF TRANSPORTATION	
PROFILE SHEET STA. 1448+00 to STA. 1463+00 "A"	

HORIZONTAL SCALE 1"=50'	
VERTICAL SCALE 1"=10'	
BRIDGE FILE DESIGNATION 0500450	
SURVEY BOOK ELECTRONIC / AERIAL CONTRACT IR-33741	
PAGE PS14	
SHEETS 50 of 66	
PROJECT 0500450	



All R/W on this sheet to be as shown.
All R/W on this sheet described from Line "A" except as noted.

LEGEND

□ - Begin L.A. R/W
□ - End L.A. R/W

CV Cave
SH Sink Hole
SPR Spring

DT Dye Tracing
SS Sinking Spring
SW Swallet

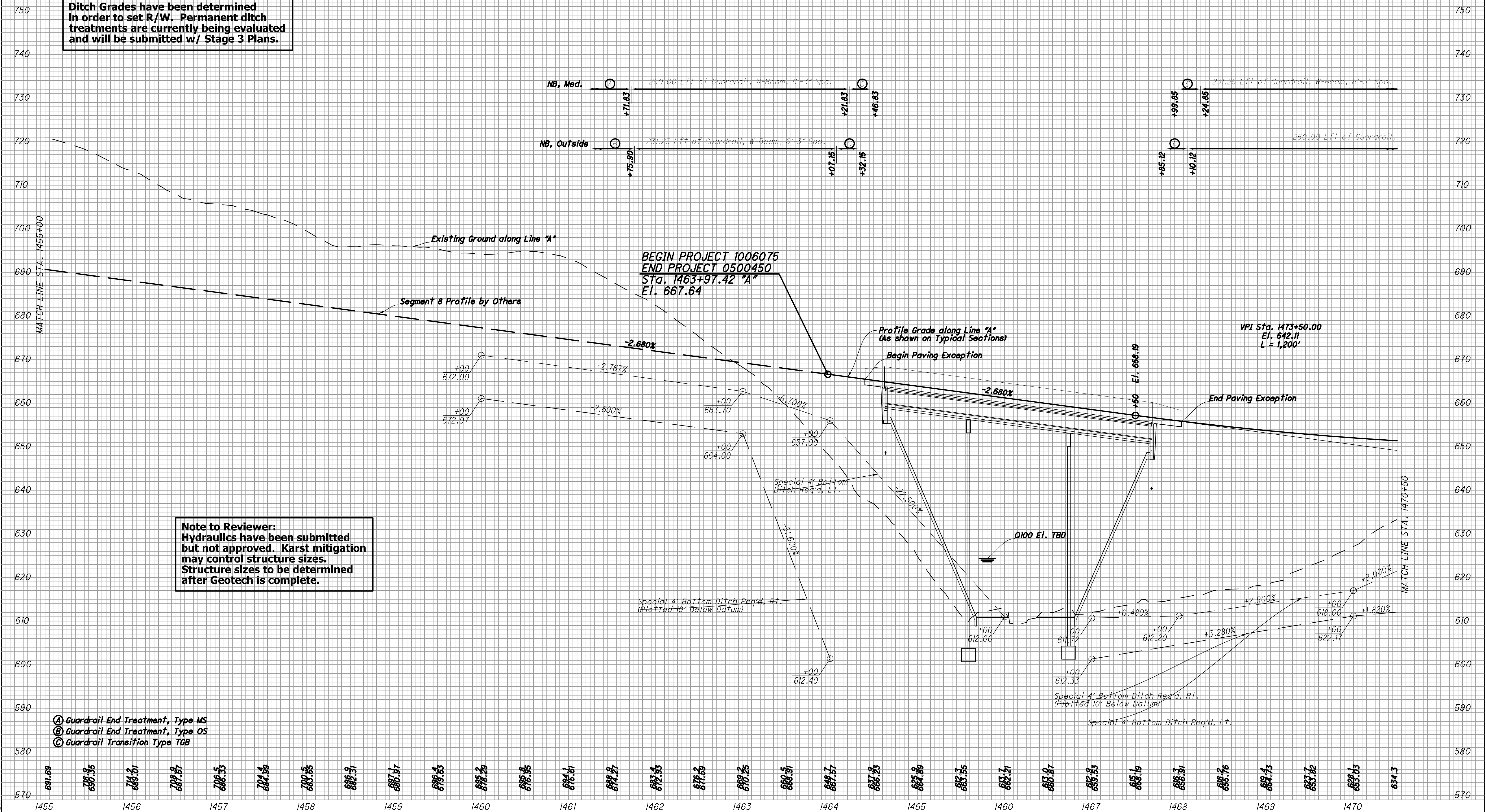
■ - Sinkhole Rim

RECOMMENDED FOR APPROVAL	
DESIGNED: MDO	DRAWN: KCH
CHECKED: HCF	CHECKED: MDO

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN SHEET	
STA. 1455+00 TO STA. 1470+50 "A"	

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	N/A
VERTICAL SCALE	DESIGNATION
NONE	1006075
SURVEY BOOK	PAGE
ELECTRONIC / AERIAL	27 of 53
CONTRACT	PROJECT
IR-33742	1006075

Note to Reviewer:
Ditch Grades have been determined
in order to set R/W. Permanent ditch
treatments are currently being evaluated
and will be submitted w/ Stage 3 Plans.



(A) Guardrail End Treatment, Type MS
 (B) Guardrail End Treatment, Type OS
 (C) Guardrail Transition Type TGB

Note to Reviewer:
Hydraulics have been submitted
but not approved. Karst mitigation
may control structure sizes.
Structure sizes to be determined
after Geotech is complete.

BEGIN PROJECT 1006075
END PROJECT 0500450
Sta. 1463+97.42 "A"
El. 667.64

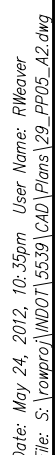
VPI Sta. 1473+50.00
El. 642.11
L = 1,200'

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: MDO	DRAWN: KCH		
CHECKED: HCF	CHECKED: MDO		

INDIANA
DEPARTMENT OF TRANSPORTATION

PROFILE SHEET
STA. 1455+00 TO STA. 1470+50 "A"

HORIZONTAL SCALE	BRIDGE FILE	
1" = 50'	N/A	
VERTICAL SCALE	DESIGNATION	
1" = 10'	1006075	
SURVEY BOOK	PAGE	SHEETS
ELECTRONIC / AERIAL	PS-04	28 of 53
CONTRACT	PROJECT	
IR-33742	1006075	



Ditch Grades have been determined in order to set R/W. Permanent ditch treatments are currently being evaluated and will be submitted w/ Stage 3 Plans.

231.25 Lft of Guardrail, W-Beam, 6'-3" Spa.
SB, Outside

250.00' Lft of Guardrail, W-Beam, 6'-3" Spa.
SB, Med.

VPI Sta. 1497+50.00
El. 719.87
L = 2,800'

VPI Sta. 1473+50.00
El. 642.11
L = 1,200'

Profile Grade along Line "A"
(As shown on Typical Sections)

Existing Ground along Line "A"

Special 4' Bottom Ditch Req'd, Lt.

Special 4' Bottom Ditch Req'd, Rt.
(Plotted 10' Below Datum)

Str. 134

Match Line STA. 1470+50

Match Line STA. 1448+50

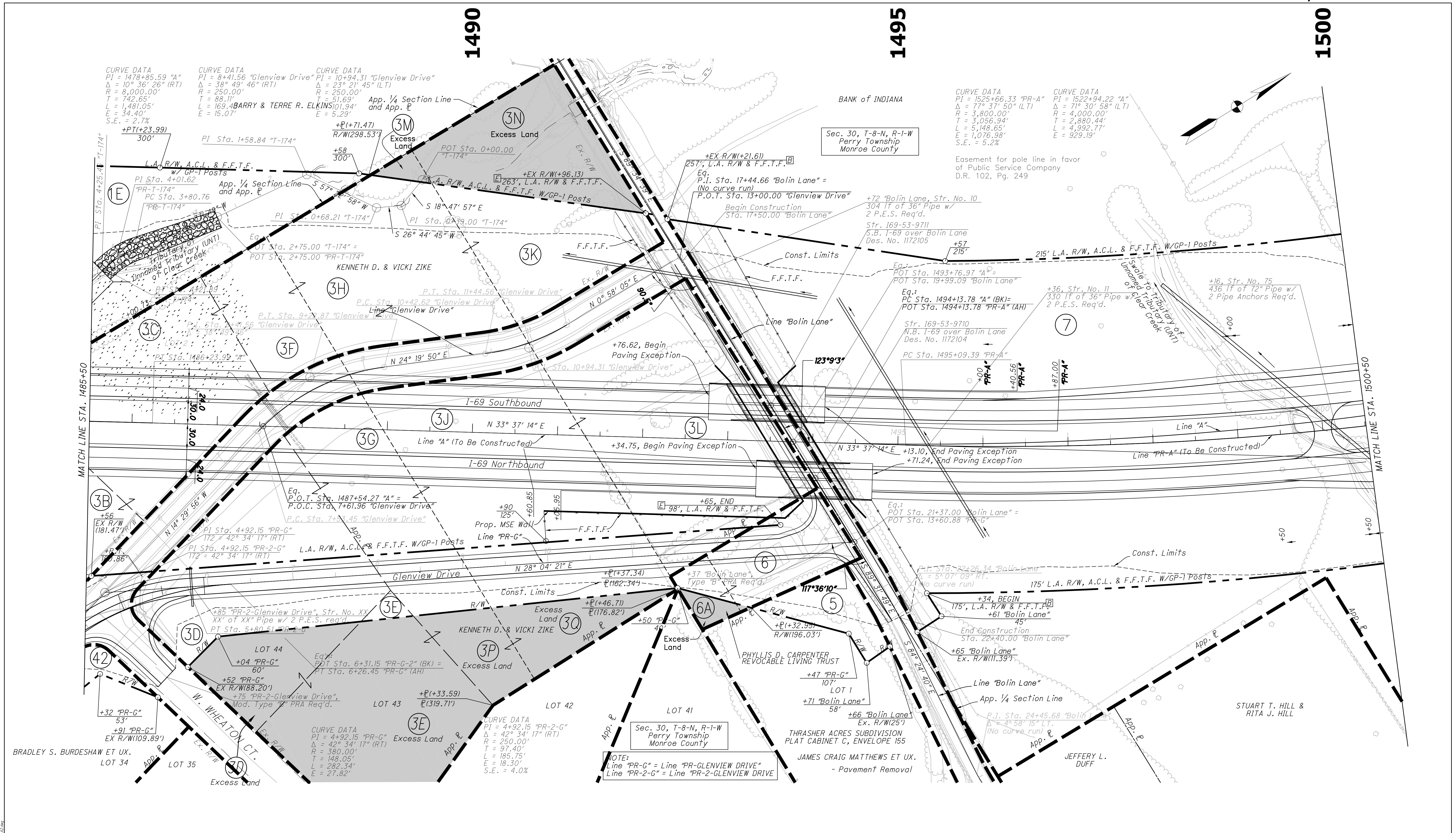
Legend:
A Guardrail End Treatment, Type MS
B Guardrail End Treatment, Type OS
C Guardrail Transition Type TGB

Stationing: 1471, 1472, 1473, 1474, 1475, 1476, 1477, 1478, 1479, 1480, 1481, 1482, 1483, 1484, 1485

Elevation: 570, 580, 590, 600, 610, 620, 630, 640, 650, 660, 670, 680, 690, 700, 710, 720, 730, 740, 750

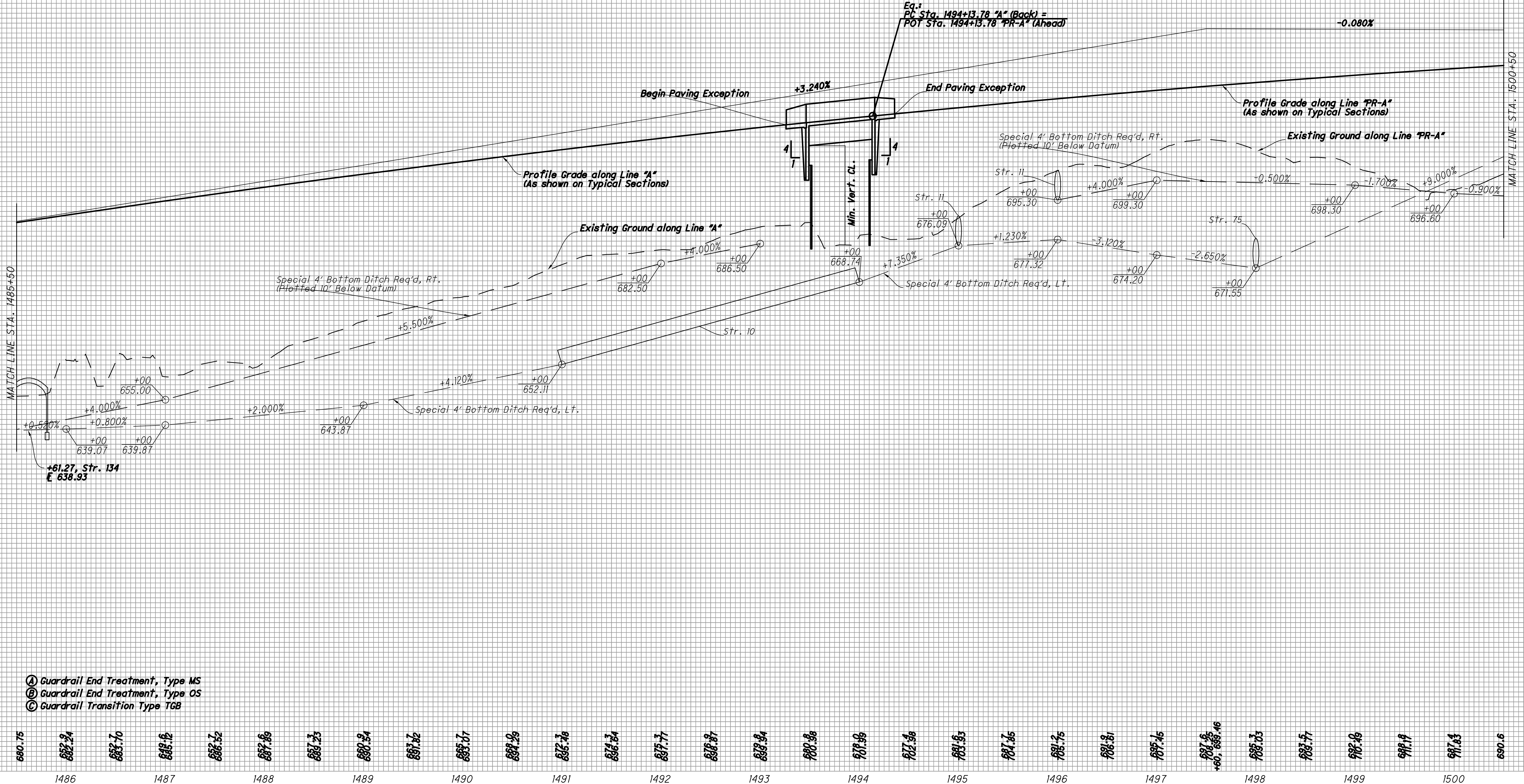
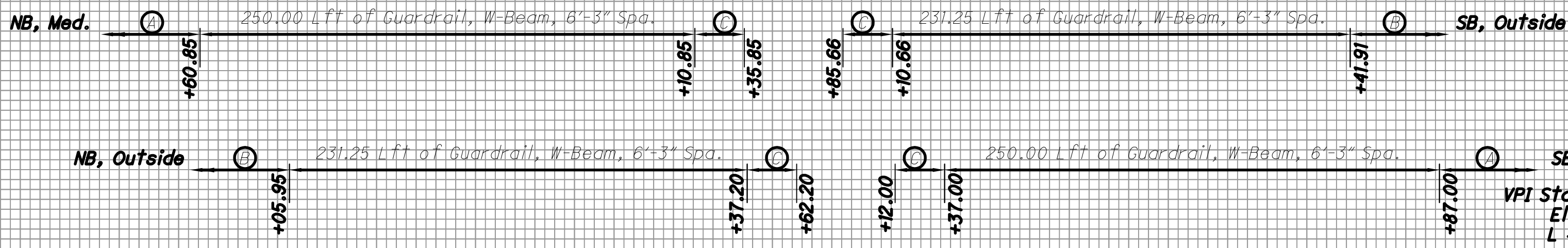
- ☐ A Guardrail End Treatment, Type MS
☐ B Guardrail End Treatment, Type OS
☐ C Guardrail Transition Type TGB

RECOMMENDED FOR APPROVAL <div>DESIGN ENGINEERDATE</div>		INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE		BRIDGE FILE			
				1" = 50'		N/A			
				VERTICAL SCALE		DESIGNATION			
				1" = 10'		1006075			
DESIGNED: MDO		DRAWN: KCH		PROFILE SHEET STA. 1470+50 TO STA. 1485+50 "A"					
CHECKED: HCF		CHECKED: MDO							
				SURVEY BOOK		PAGE		SHEETS	
				ELECTRONIC / AERIAL		PS-05		30 of 53	
				CONTRACT		PROJECT			
				IR-33742		1006075			



All R/W on this sheet to be as shown. All R/W on this sheet described from Line "A" except as noted. <u>LEGEND</u> [B] - Begin L.A. R/W [E] - End L.A. R/W	<div>CV</div> Cave	<div>DT</div> Dye Tracing	<div>SS</div> Sinking Spring	<div>SW</div> Swallet	<div></div> - Sinkhole Rim		RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE	
	VERTICAL SCALE	DESIGNATION					1" = 50'	N/A					
	NONE	1006075											
	PLAN SHEET												
STA. 1485+50 "A" TO STA. 1500+50 "PR-A"										SURVEY BOOK		PAGE	SHEETS
										ELECTRONIC / AERIAL		PP-06	31 of 53
										CONTRACT		PROJECT	
										IR-33742		1006075	

Note to Reviewer:
Ditch Grades have been determined
in order to set R/W. Permanent ditch
treatments are currently being evaluated
and will be submitted w/ Stage 3 Plans.



- ☐ (A) Guardrail End Treatment, Type MS
☐ (B) Guardrail End Treatment, Type OS
☐ (C) Guardrail Transition Type TGB

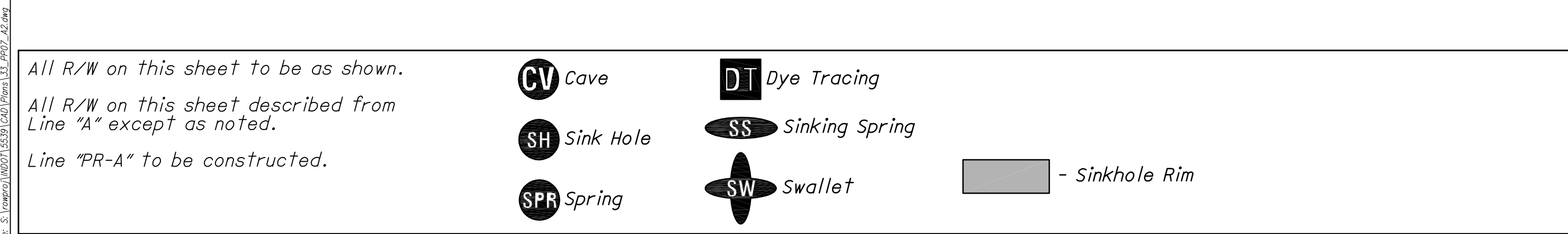
RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: MDO	DRAWN: KCH		
CHECKED: HCF	CHECKED: MDO		

**INDIANA
DEPARTMENT OF TRANSPORTATION**

PROFILE SHEET

STA. 1485+50 "A" TO STA. 1500+50 "PR-A"

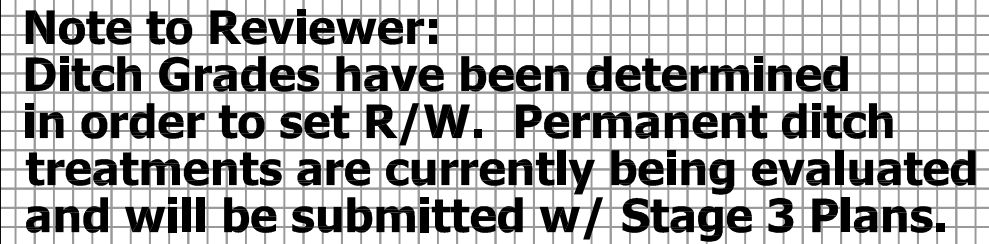
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VERTICAL SCALE	DESIGNATION	
1" = 10'	1006075	
SURVEY BOOK	PAGE	SHEETS
ELECTRONIC / AERIAL	PS-06	32 of 53
CONTRACT	PROJECT	
10-33742	1006075	



RECOMMENDED FOR APPROVAL		DESIGN ENGINEER	DATE
DESIGNED: MDO	DRAWN: KCH		
CHECKED: HCF	CHECKED: MDO		

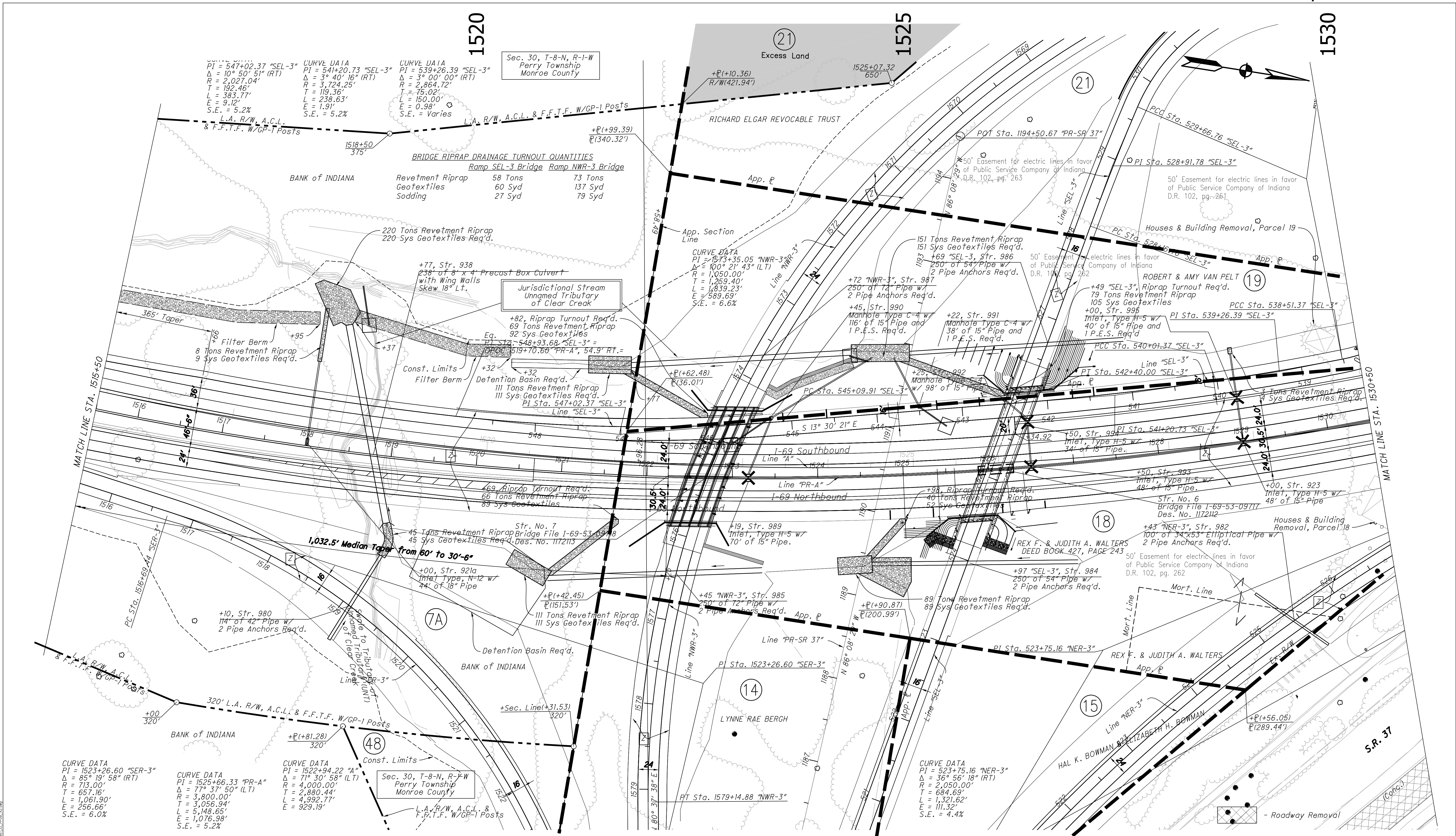
INDIANA DEPARTMENT OF TRANSPORTATION
<i>PLAN SHEET</i> <i>STA. 1500+50 TO STA. 1515+50 "PR-A"</i>

HORIZONTAL SCALE	BRIDGE FILE	
1" = 50'	N/A	
VERTICAL SCALE	DESIGNATION	
NONE	1006075	
SURVEY BOOK	PAGE	SHEETS
ELECTRONIC / AERIAL	PP-07	33 of 53
CONTRACT	PROJECT	
IR-33742	1006075	



<p align="center">INDIANA DEPARTMENT OF TRANSPORTATION</p>
<p align="center"><i>PROFILE SHEET</i> <i>STA. 1500+50 TO STA. 1515+50 "PR-A"</i></p>

HORIZONTAL SCALE	BRIDGE FILE	
1" = 50'	N/A	
VERTICAL SCALE	DESIGNATION	
1" = 10'	1006075	
SURVEY BOOK	PAGE	SHEETS
ELECTRONIC / AERIAL	PS-07	34 of 53
CONTRACT	PROJECT	
IR-33742	1006075	



LEGEND

- SH Sinkhole
- SPR Spring
- SW Swallet

Do Not Disturb Trees

② See Typical Sections for Construction Materials
(From 1462+50 "A" to 1522+25 "PR-A", use Rural Typical Sections.
From 1522+25 "PR-A" to 1553+25 "PR-A", use Urban Typical Sections.)

Note:
All R/W on this sheet to be as shown.
All R/W on this sheet described from Line "A" except as noted.
Line "PR-A" to be constructed.

RECOMMENDED FOR APPROVAL

DESIGNED: MDO DRAWN: KCH

CHECKED: HCF CHECKED: MDO

INDIANA

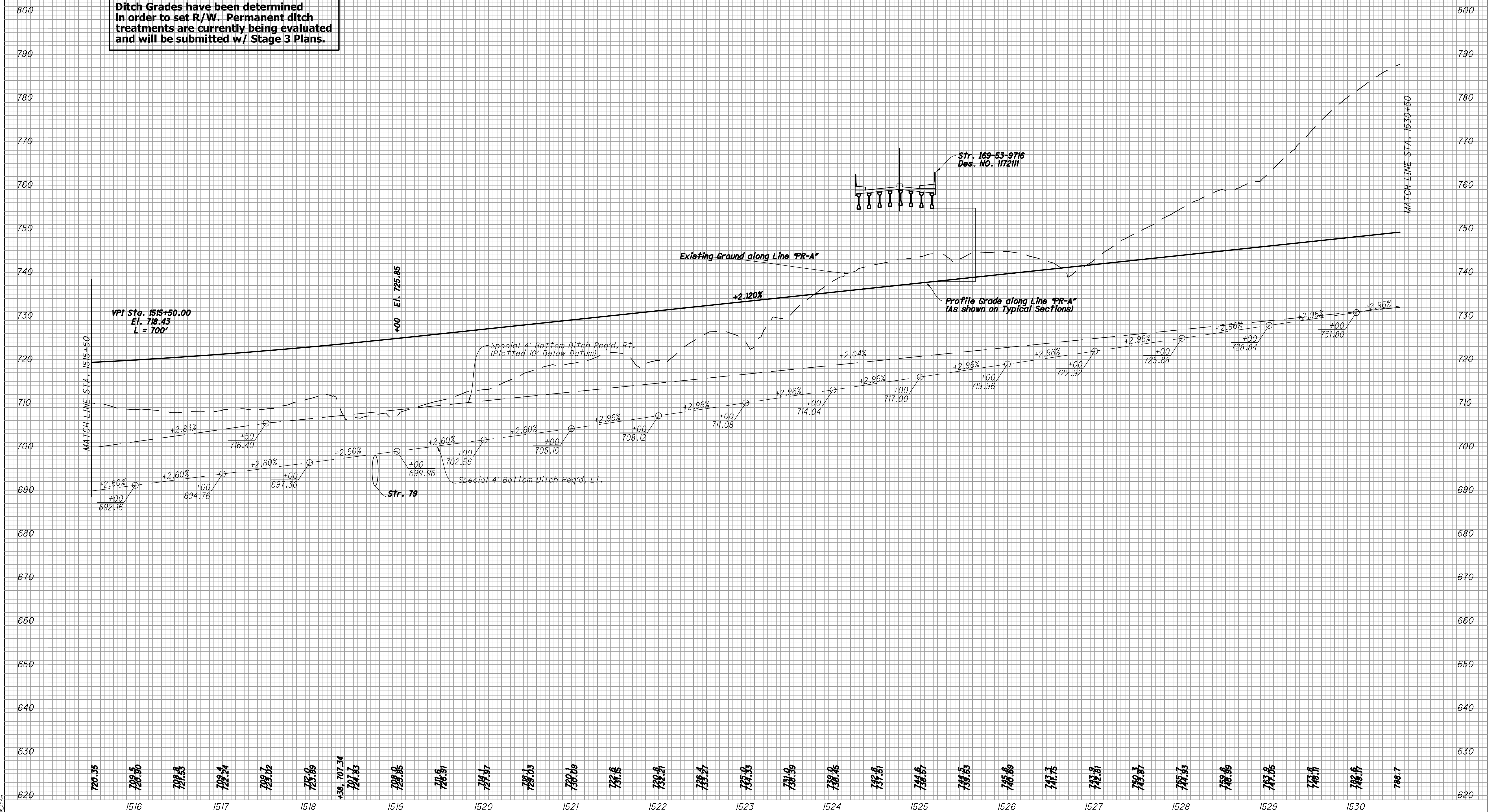
DEPARTMENT OF TRANSPORTATION

PLAN SHEET

STA. 1515+50 TO STA. 1530+50 "PR-A"

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	N/A
VERTICAL SCALE	DESIGNATION
NONE	1006075
SURVEY BOOK	PAGE
ELECTRONIC / AERIAL	PP-08
CONTRACT	PROJECT
IR-33742	1006075

Note to Reviewer:
Ditch Grades have been determined
in order to set R/W. Permanent ditch
treatments are currently being evaluated
and will be submitted w/ Stage 3 Plans.

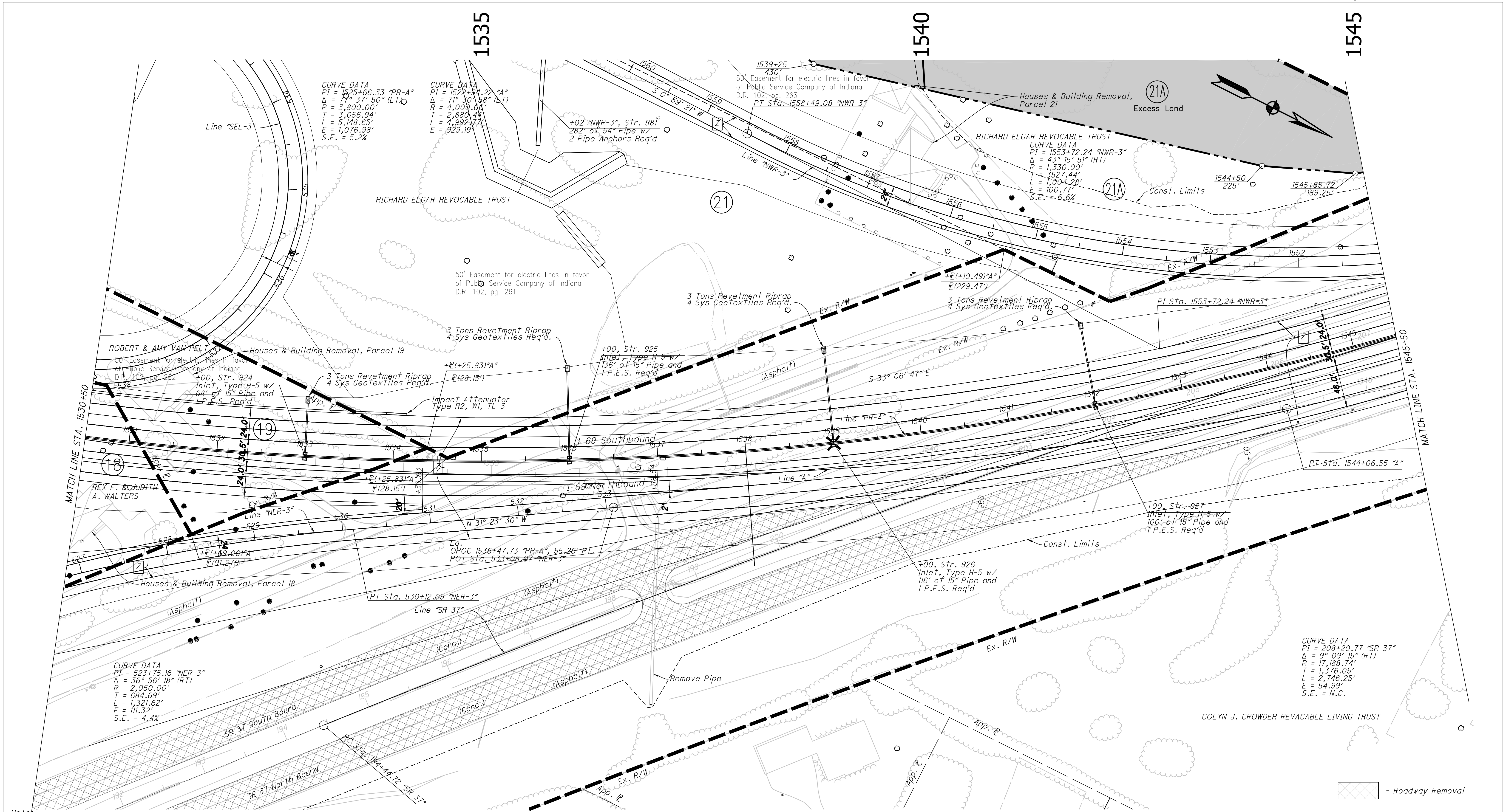


RECOMMENDED FOR APPROVAL		_____ DESIGN ENGINEER	_____ DATE
DESIGNED: _____ MDO	DRAWN: _____ KCH		
CHECKED: _____ HCF	CHECKED: _____ MDO		

**INDIANA
DEPARTMENT OF TRANSPORTATION**

PROFILE SHEET
STA. 1515+50 TO STA. 1530+50 "PR-A"

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	N/A
VERTICAL SCALE	DESIGNATION
1" = 10'	1006075
SURVEY BOOK	PAGE SHEETS
ELECTRONIC / AERIAL	PS-08 / 36 of 53
CONTRACT	PROJECT
IR-33742	1006075



Note:
All R/W on this sheet to be as shown.

All R/W on this sheet described from
Line "A" except as noted.

Line "PR-A" to be constructed.

LEGEND



Sinkhole



See Typical Sections for Construction Materials

For Sinkhole Treatment Detail See Sheet 100.

RECOMMENDED
FOR APPROVAL
DESIGN ENGINEER
DATE

DESIGNED: MDO DRAWN: KCH

CHECKED: HCF CHECKED: MDO

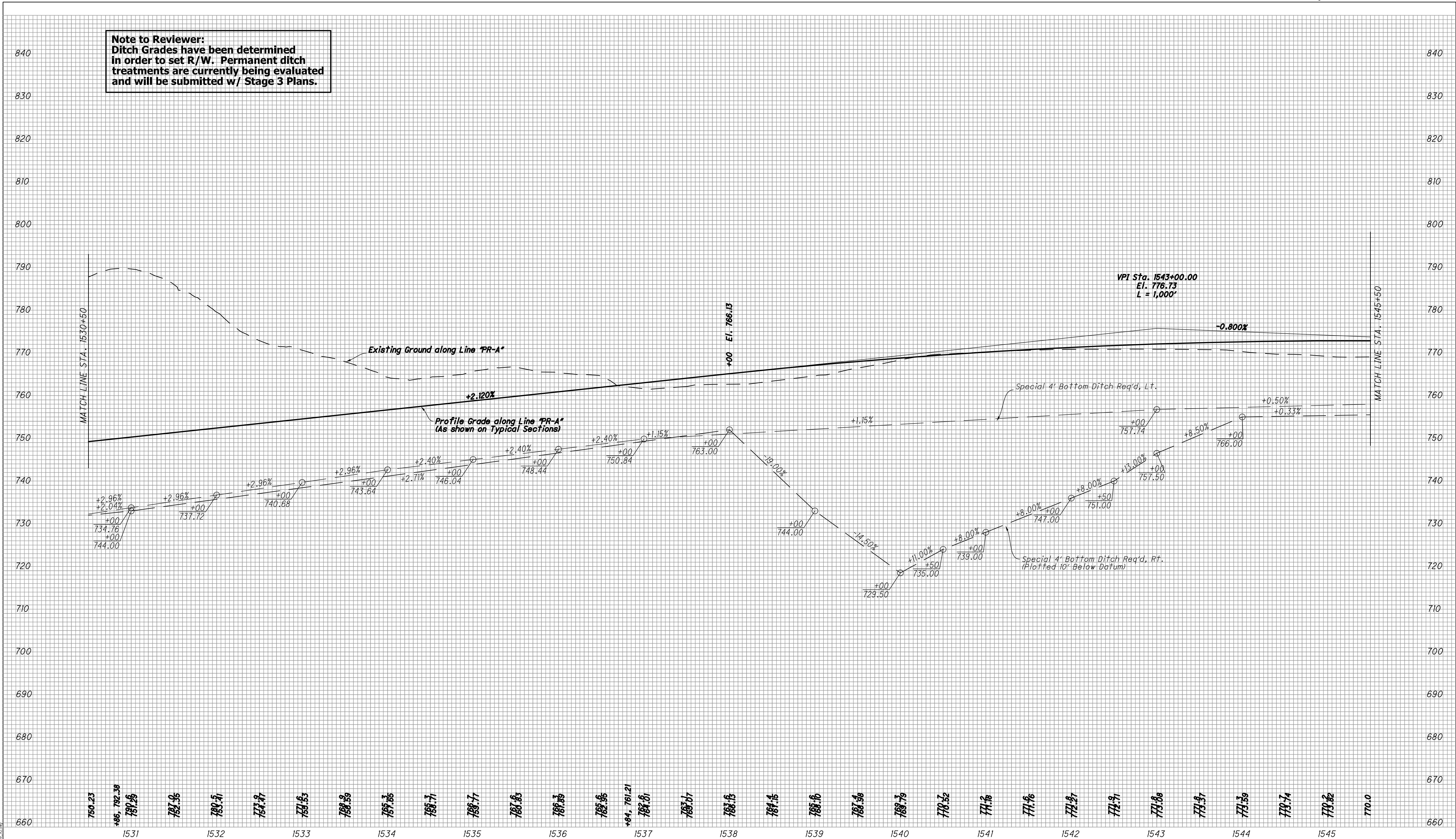
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 1530+50 TO STA. 1545+50 "PR-A"

HORIZONTAL SCALE
1" = 50'
VERTICAL SCALE
NONE
BRIDGE FILE
N/A
DESIGNATION
1006075

SURVEY BOOK
ELECTRONIC / AERIAL
CONTRACT
IR-33742
PAGE
PP-09
PROJECT
1006075
SHEETS
37 of 53

Note to Reviewer:
Ditch Grades have been determined
in order to set R/W. Permanent ditch
treatments are currently being evaluated
and will be submitted w/ Stage 3 Plans.

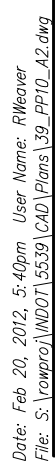


DESIGNED: MDO	DRAWN: KCH
CHECKED: HCF	CHECKED: MDO

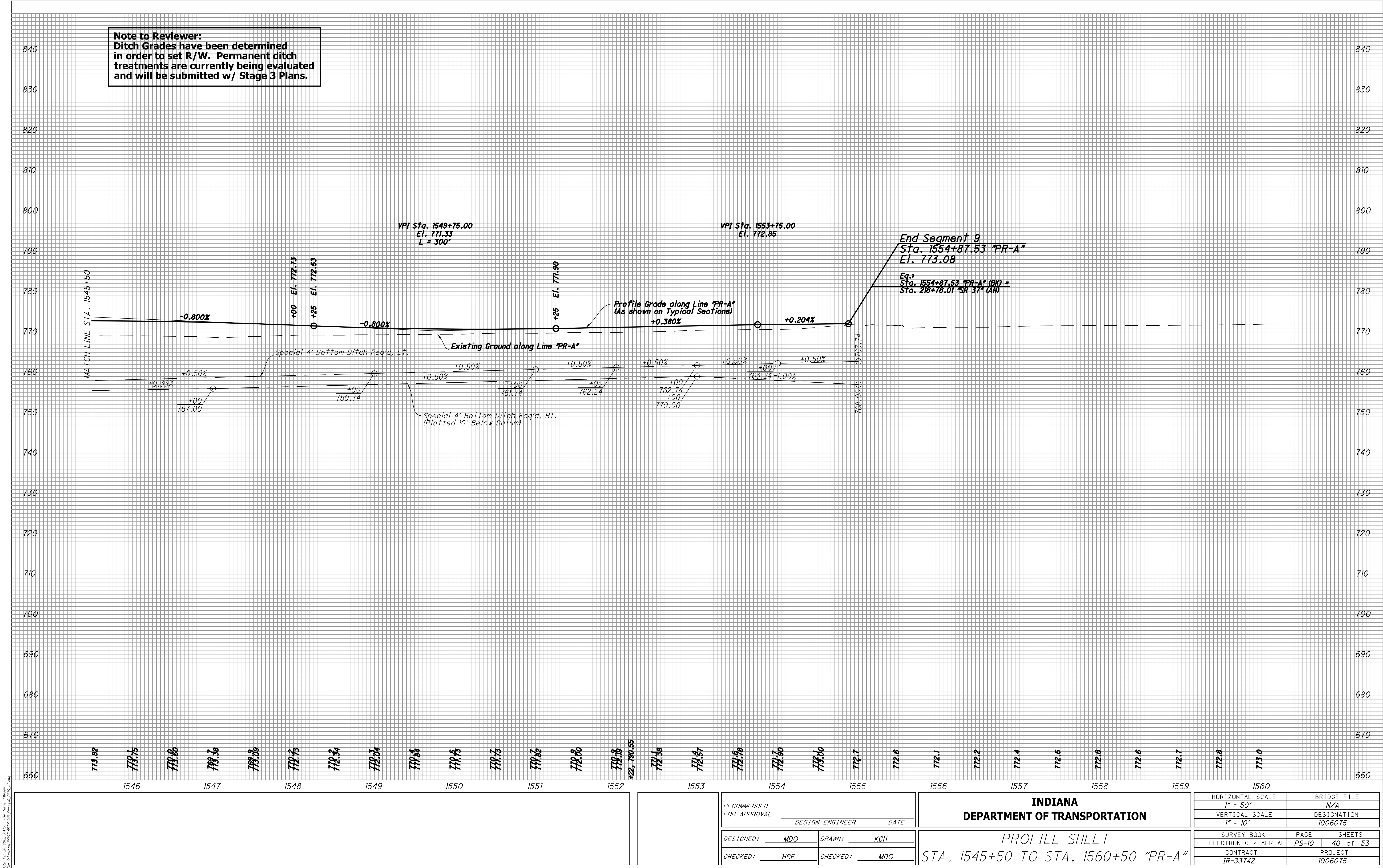
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: MDO	DRAWN: KCH	
CHECKED: HCF	CHECKED: MDO	

INDIANA DEPARTMENT OF TRANSPORTATION
PROFILE SHEET STA. 1530+50 TO STA. 1545+50 "PR-A"

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE N/A
VERTICAL SCALE 1" = 10'	DESIGNATION 1006075
SURVEY BOOK ELECTRONIC / AERIAL	PAGE PS-09
CONTRACT IR-33742	SHEETS 38 of 53
	PROJECT 1006075



Note to Reviewer:
Ditch Grades have been determined
in order to set R/W. Permanent ditch
treatments are currently being evaluated
and will be submitted w/ Stage 3 Plans.

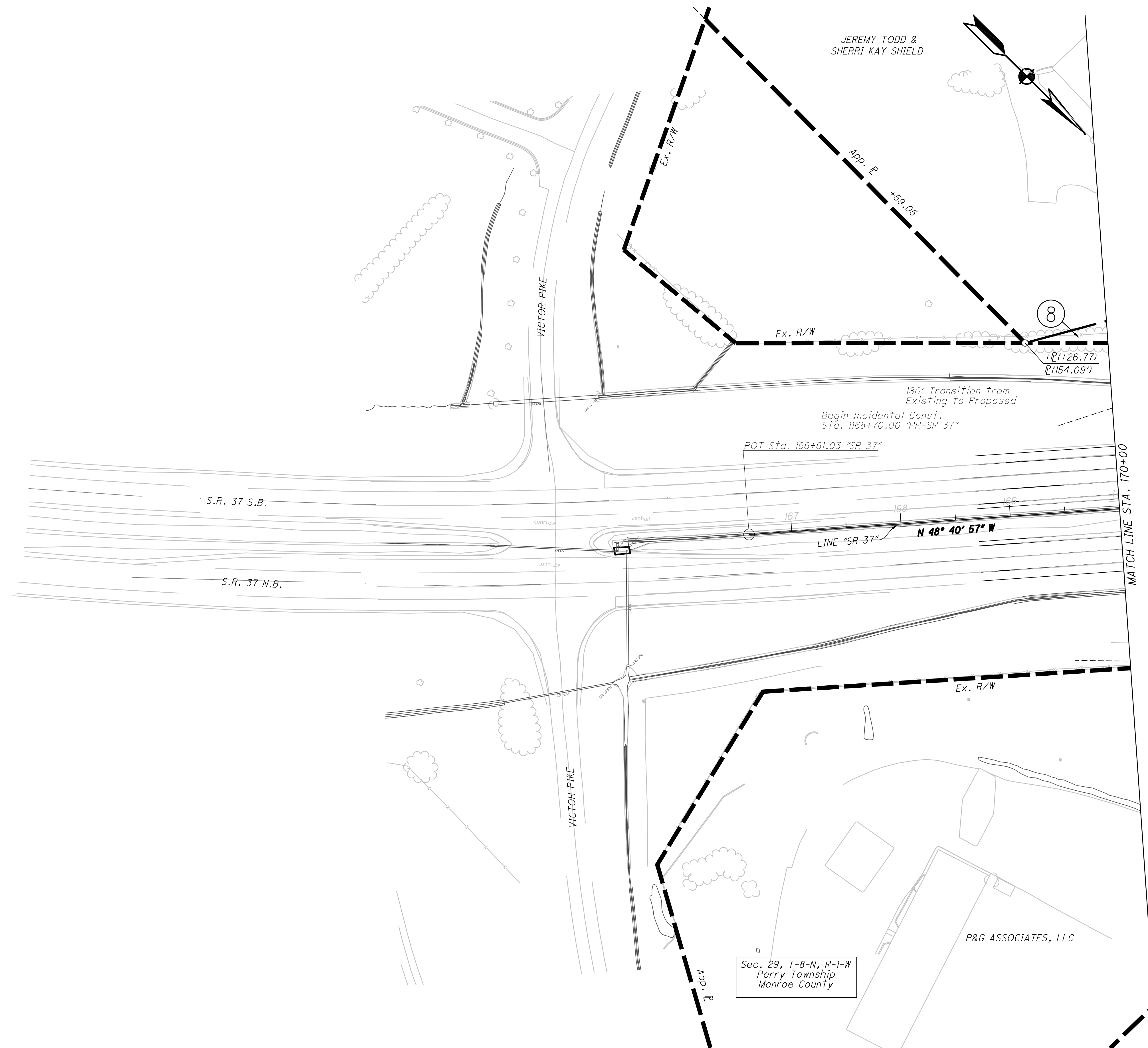


Drawn: 10/20/2017 10:00 AM
Checked: 10/20/2017 10:00 AM
Date: 10/20/2017 10:00 AM

RECOMMENDED FOR APPROVAL		DESIGN ENGINEER		DATE	
DESIGNED: MDO		DRAWN: KCH			
CHECKED: HCF		CHECKED: MDO			

INDIANA DEPARTMENT OF TRANSPORTATION	
PROFILE SHEET STA. 1545+50 TO STA. 1560+50 "PR-A"	

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE N/A
VERTICAL SCALE 1" = 10'	DESIGNATION 1006075
SURVEY BOOK ELECTRONIC / AERIAL	PAGE PS-10
CONTRACT IR-33742	SHEETS 40 of 53



All R/W on this sheet to be as shown.

All R/W on this sheet described from
Line "SR 37" except as noted.

Line "PR-SR 37" to be constructed.

CV *Cave*

SH Sink Hole

 SPR Spring

DT *Dye Tracing*

SS *Sinking Spring*

SW *Swallet*

- Sinkhole Rim

RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
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DESIGNED: MDO	DRAWN: KCH
---------------	------------

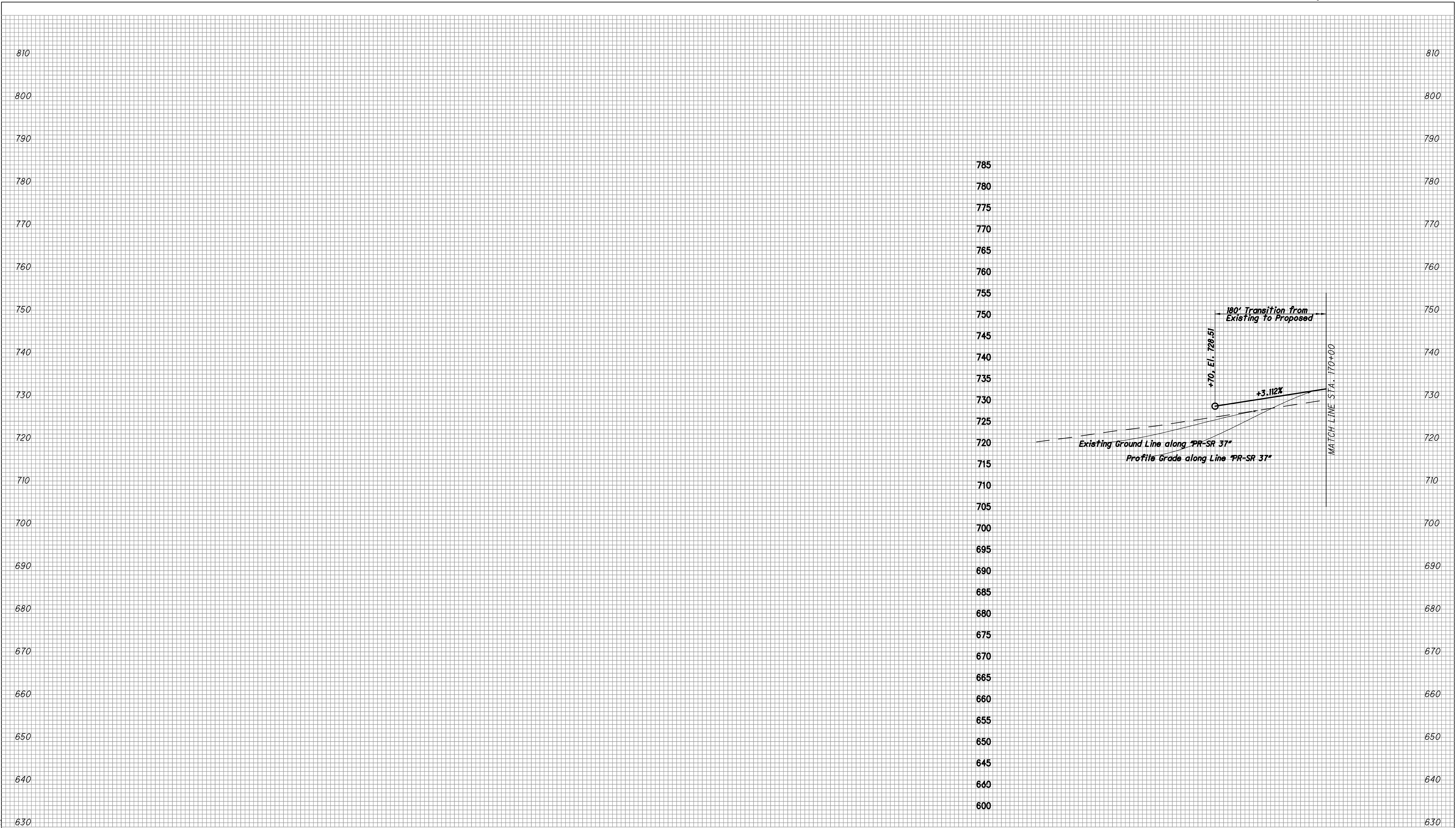
CHECKED:	HCF	CHECKED:	MDO
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**INDIANA
DEPARTMENT OF TRANSPORTATION**

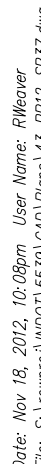
PLAN SHEET
STA. 166+61.03 TO STA. 170+00.00 "SR37"

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1006075

SURVEY BOOK	PAGE	SHEETS
ELECTRONIC / AERIAL	PP-11	41 of 53
CONTRACT	PROJECT	
IR-33742	1006075	



		RECOMMENDED FOR APPROVAL _____ <div style="text-align: right; margin-top: -10px;">DESIGN ENGINEER DATE</div>	167 168 169 170 INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 50'	BRIDGE FILE N/A
		DESIGNED: _____ MDO DRAWN: _____ KCH	PROFILE SHEET STA. 166+61.03 TO STA. 170+00 "SR37"	VERTICAL SCALE 1" = 10'	DESIGNATION 1006075
		CHECKED: _____ HCF CHECKED: _____ MDO		SURVEY BOOK ELECTRONIC / AERIAL	PAGE SHEETS PS-11 42 of 53
				CONTRACT IR-33742	PROJECT 1006075



Note: Nov 18, 2012, 10:08pm User Name: RHWOWR

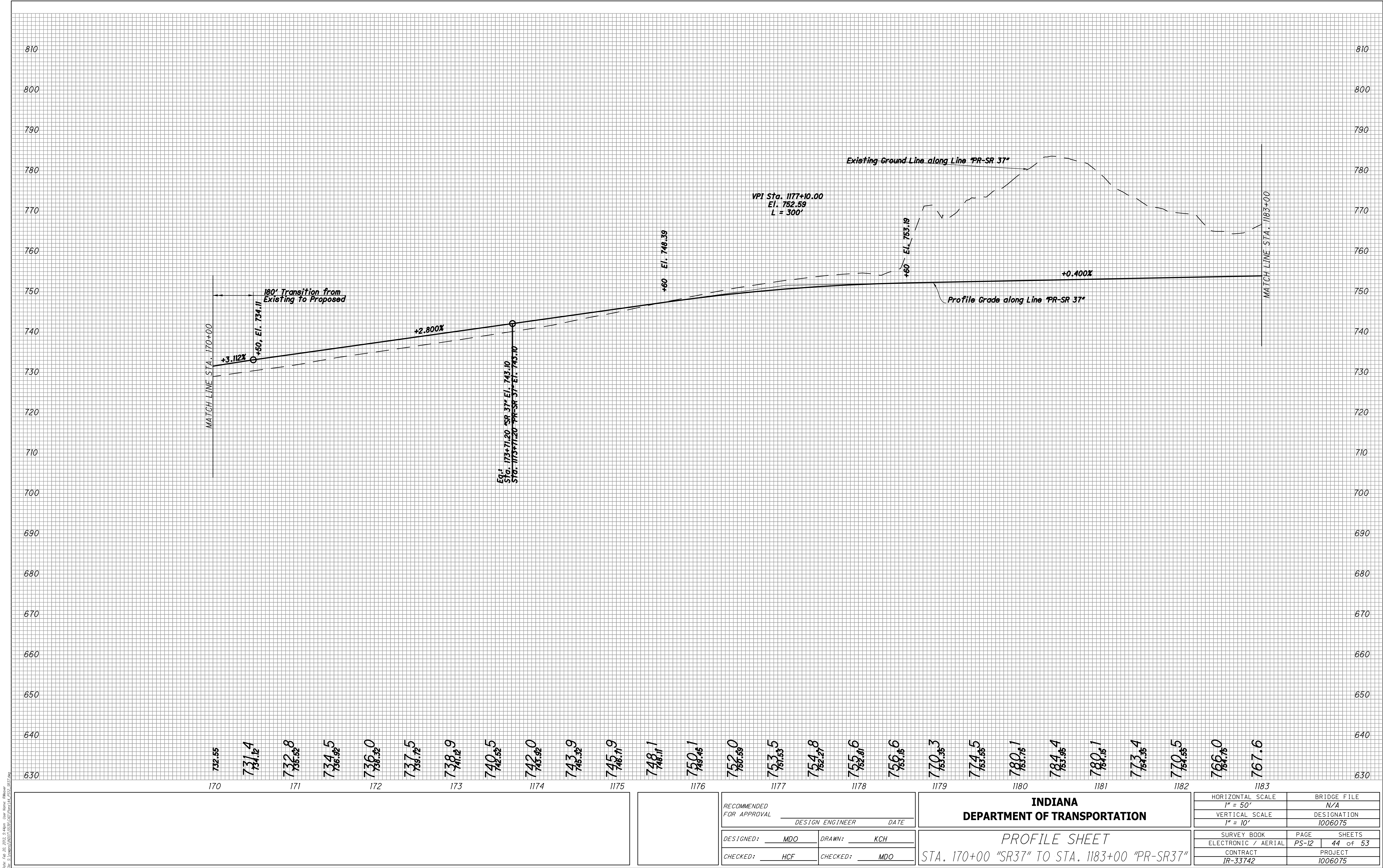
Note: Nov 18, 2012, 10:08pm User Name: RHWOWR

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Note: Nov 18, 2012, 10:08pm User Name: RHWOWR

Note: Nov 18, 2012, 10:08pm User Name: RHWOWR



Drawn: 10/20/2017 10:55:11 AM
Date: 10/20/2017 10:55:11 AM
File: 1006075.dwg
User: KCH
Plot: 10/20/2017 10:55:11 AM
Printer: HP DesignJet T1100e

RECOMMENDED FOR APPROVAL	
DESIGNED: MDO	DRAWN: KCH
CHECKED: HCF	CHECKED: MDO

INDIANA DEPARTMENT OF TRANSPORTATION	
PROFILE SHEET	
STA. 170+00 "SR37" TO STA. 1183+00 "PR-SR37"	

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE N/A
VERTICAL SCALE 1" = 10'	DESIGNATION 1006075
SURVEY BOOK ELECTRONIC / AERIAL	PAGE PS-12
CONTRACT IR-33742	SHEETS 44 of 53



All R/W on this sheet to be as shown.
All R/W on this sheet described from Line "PR-A" except as noted.
Line "PR-SR 37" to be constructed.

CV Cave	DT Dye Tracing
SH Sink Hole	SS Sinking Spring
SPR Spring	SW Swallet
	- Sinkhole Rim

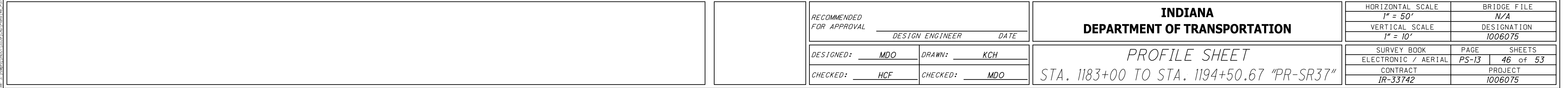
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DESIGNED: MDO	DRAWN: KCH
CHECKED: HCF	CHECKED: MDO

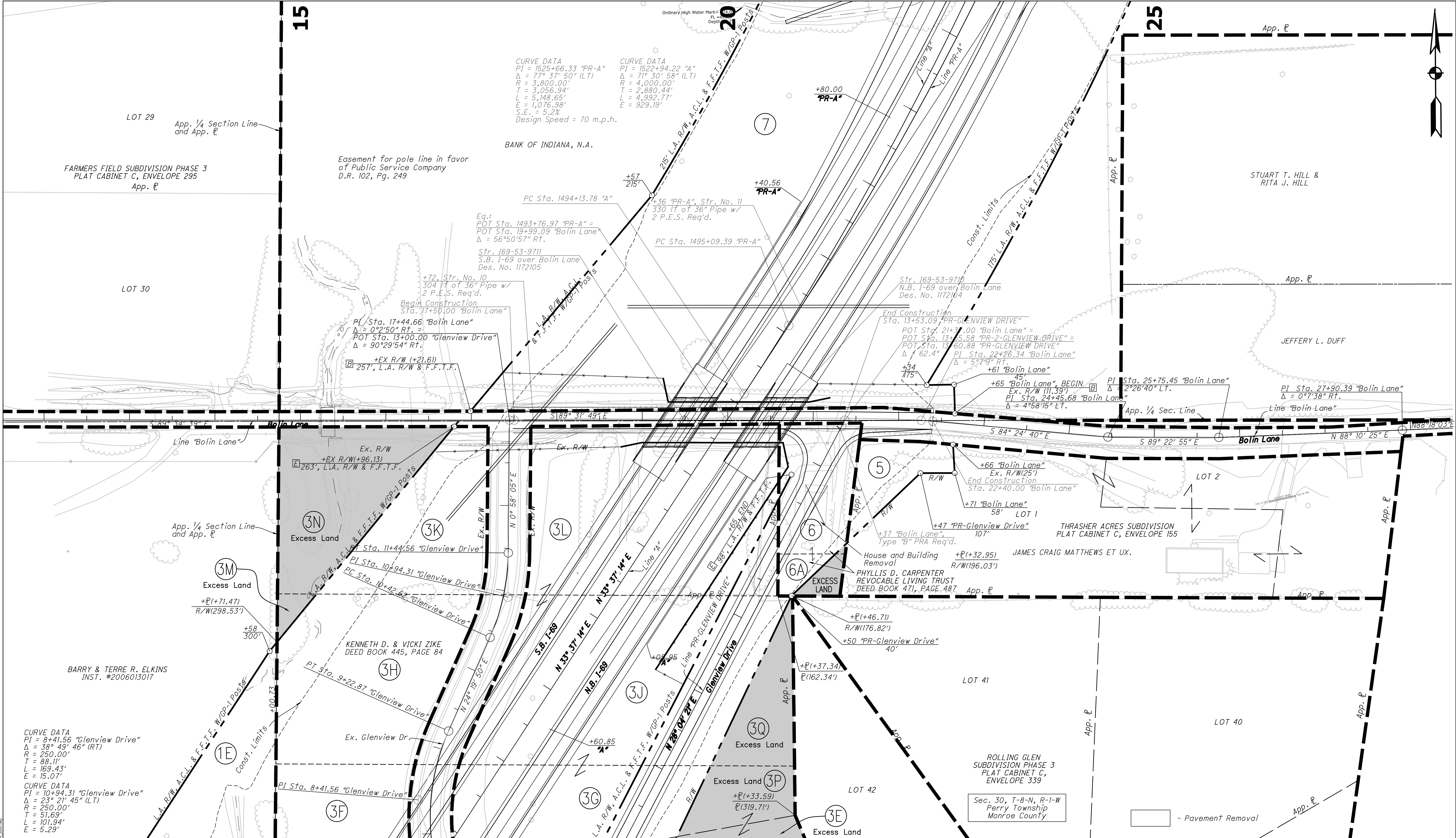
INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET

STA. 1183+00.00 TO STA. 1194+50.67 "PR-SR37"

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1006075
SURVEY BOOK ELECTRONIC / AERIAL	PAGE PP-13
CONTRACT IR-33742	SHEETS 45 of 53
	PROJECT 1006075





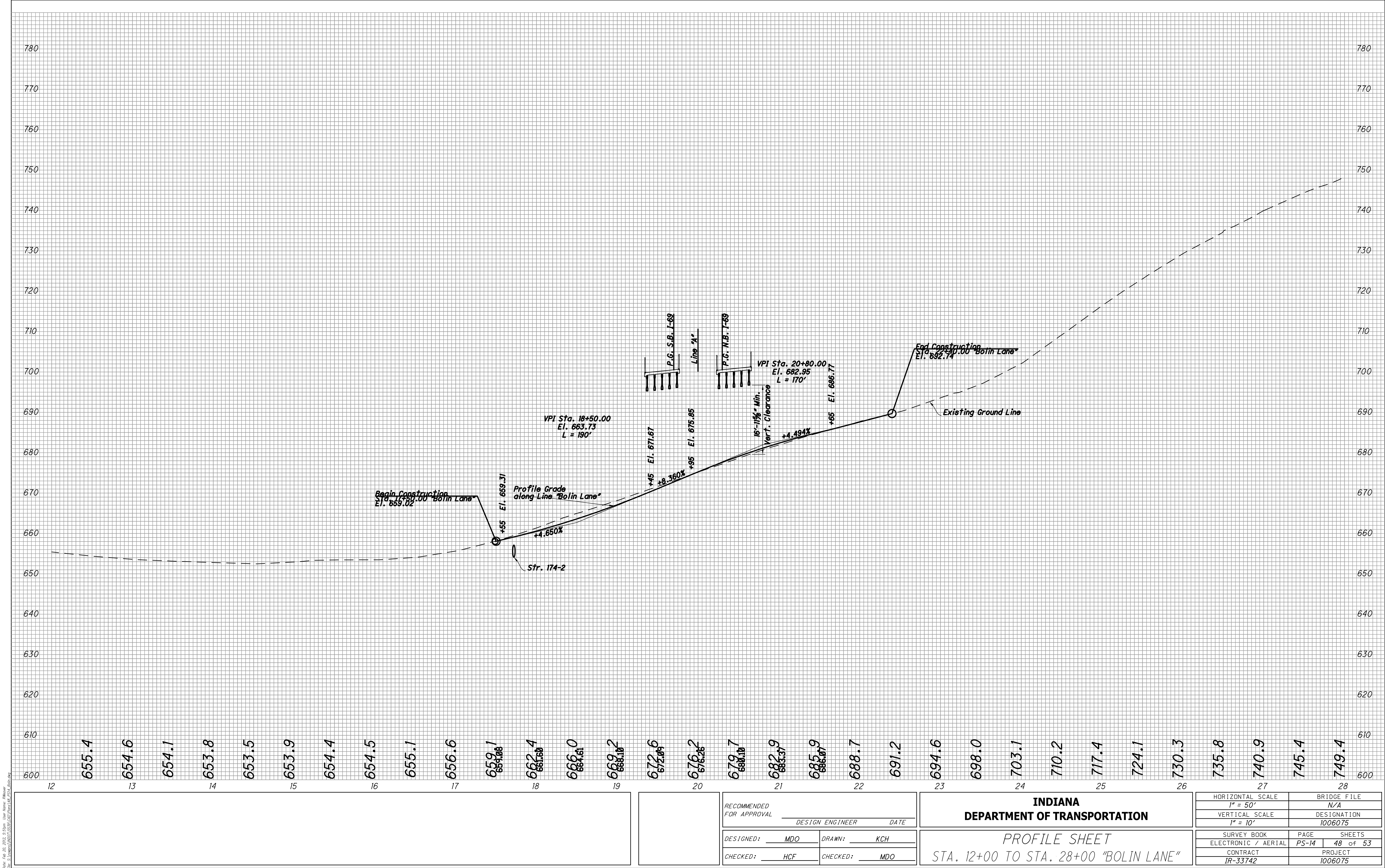
All R/W on this sheet to be as shown.
All R/W on this sheet described from
Line "A" except as noted.
Line "Bolin Lane" to be constructed.

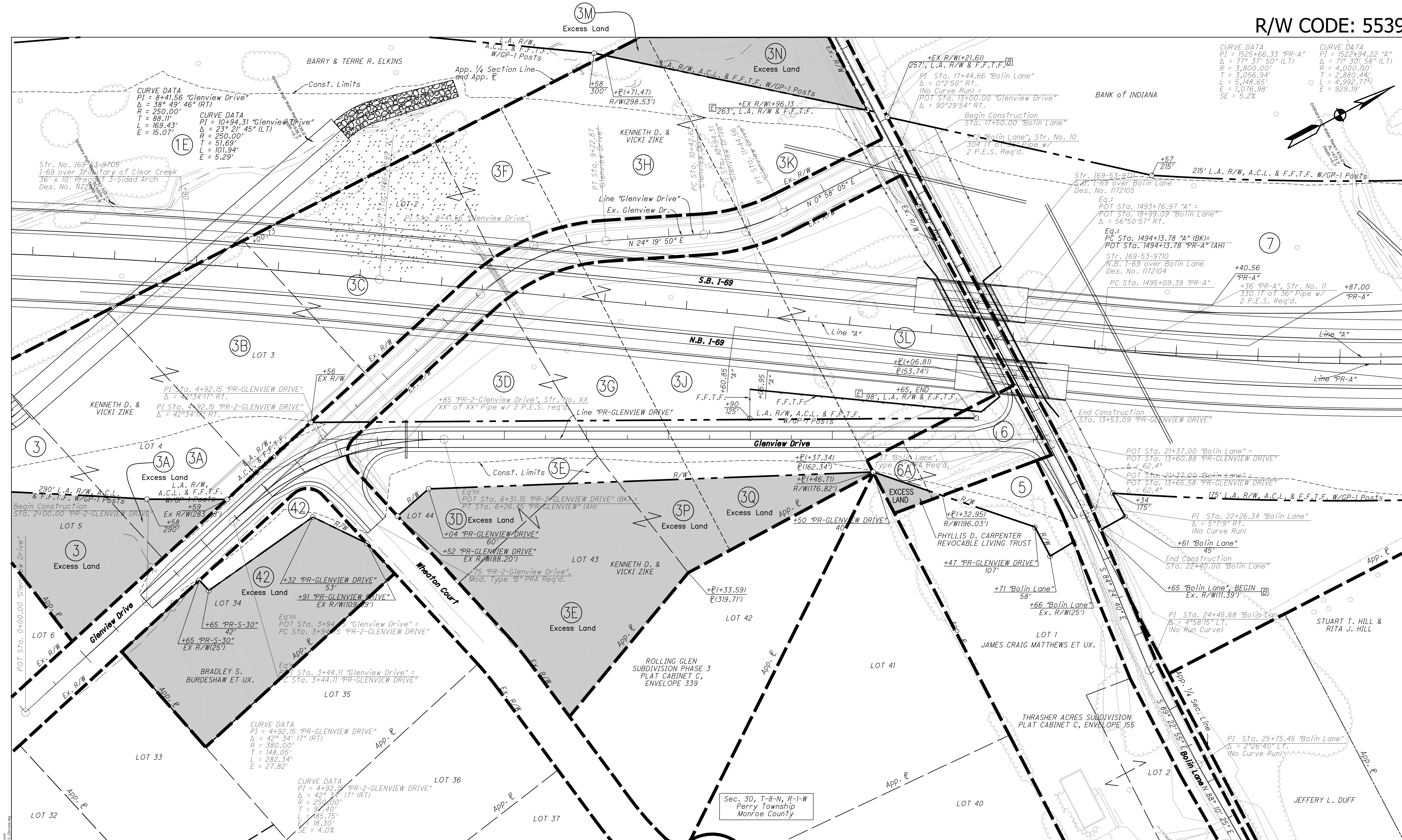
- CV Cave
SH Sink Hole
SPR Spring
DT Dye Tracing
SS Sinking Spring
SW Swallet
- Sinkhole Rim

RECOMMENDED FOR APPROVAL	
DESIGNED: MDO	DRAWN: KCH
CHECKED: HCF	CHECKED: MDO

INDIANA DEPARTMENT OF TRANSPORTATION	
PLAN SHEET STA. 12+00 TO STA. 28+00 "BOLIN LANE"	

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE N/A
VERTICAL SCALE N/A	DESIGNATION 1006075
SURVEY BOOK ELECTRONIC / AERIAL	PAGE PP-14
CONTRACT IR-33742	SHEETS 47 of 53 PROJECT 1006075





All R/W on this sheet to be as shown.

All R/W on this sheet described from Line "A" except as noted.

Line "PR-2-GLENVIEW DRIVE" to be constructed.

CV Cave
SH Sink Hole
SPR Spring

DT Dye Tracing
SS Sinking Spring
SW Swallet

— Sinkhole Rim

RECOMMENDED FOR APPROVAL

DESIGN ENGINEER DATE

DESIGNED: MDO DRAWN: KCH

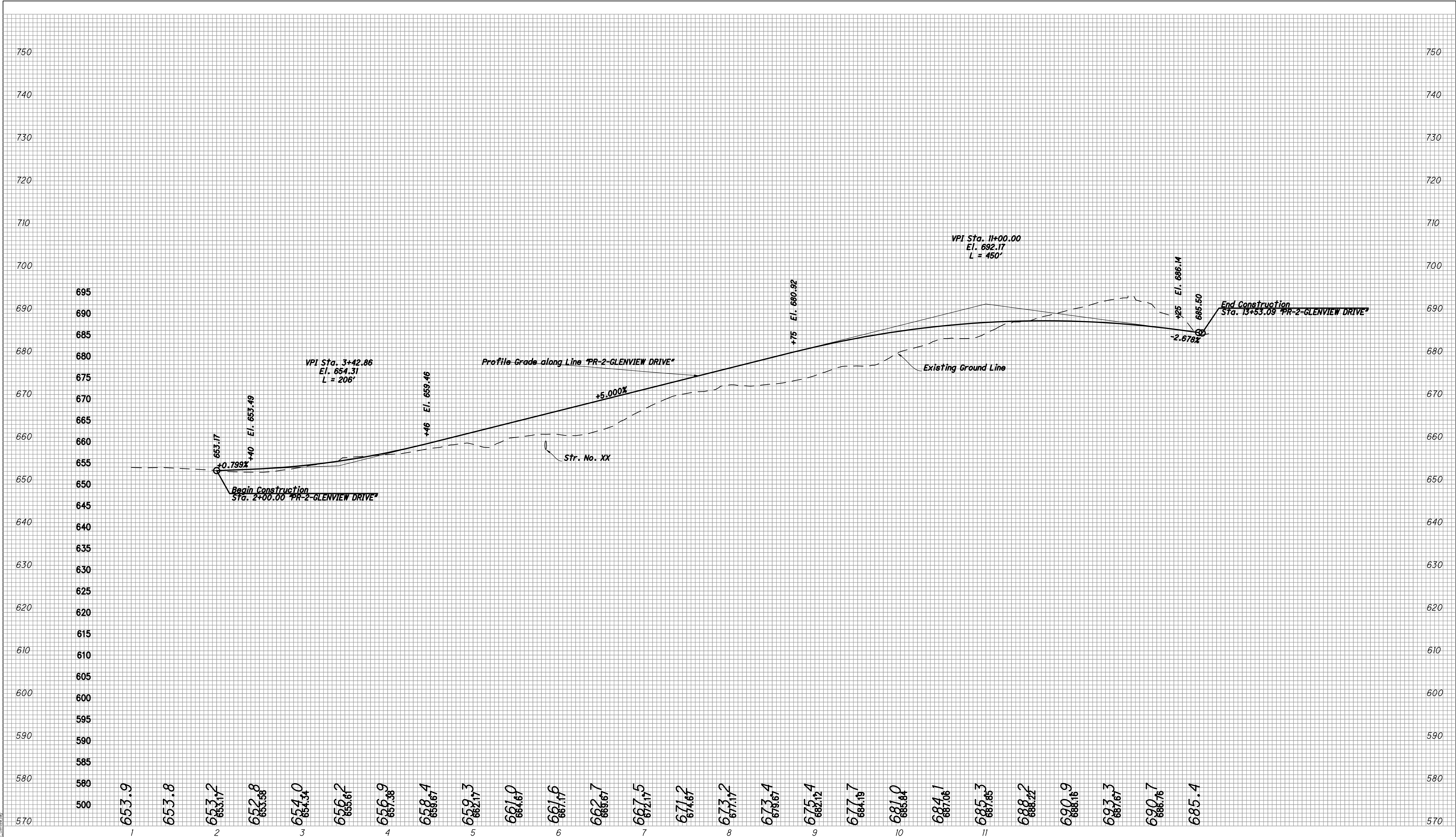
CHECKED: HCF CHECKED: MDO

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET

STA. 2+00 TO STA. 13+60.88 "PR-2-GLENVIEW DRIVE"

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	N/A
VERTICAL SCALE	DESIGNATION
N/A	1006075
SURVEY BOOK	PAGE
ELECTRONIC / AERIAL	PP-15
CONTRACT	PROJECT
IR-33742	1006075



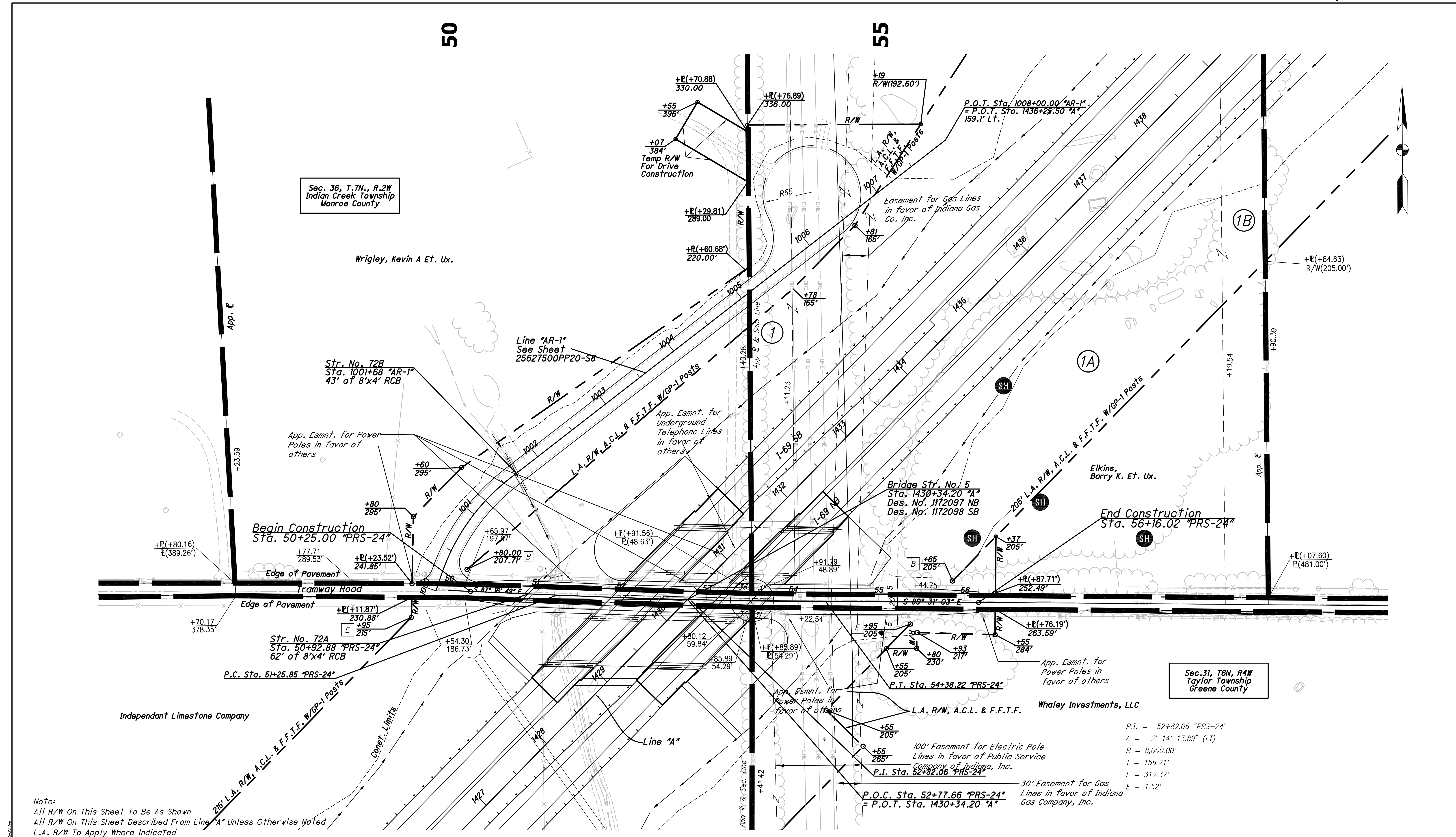
		RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 50'	BRIDGE FILE N/A		
		DESIGNED: MDO DRAWN: KCH		VERTICAL SCALE 1" = 10'	DESIGNATION 1006075		
		CHECKED: HCF CHECKED: MDO		SURVEY BOOK ELECTRONIC / AERIAL		PAGE PS-15	SHEETS 50 of 53
		CONTRACT IR-33742			PROJECT 1006075		

PROFILE SHEET	
STA. 2+00 TO STA. 13+60.88 "PR-2-GLENVIEW DRIVE"	

Drawn by: J. B. 10/15/11 11:15 AM
Checked by: J. B. 10/15/11 11:15 AM
Date: 10/15/11 11:15 AM
Project: 1006075
Sheet: 50 of 53

50

55



Note:
All R/W On This Sheet To Be As Shown
All R/W On This Sheet Described From Line "A" Unless Otherwise Noted
L.A. R/W To Apply Where Indicated

Legend

- [B] Beginning L.A. R/W
- [E] Ending L.A. R/W
- [SH] Sinkhole

RECOMMENDED
FOR APPROVAL

DESIGN ENGINEER DATE

DESIGNED: DCK

DRAWN: ETD

CHECKED: A.J.D

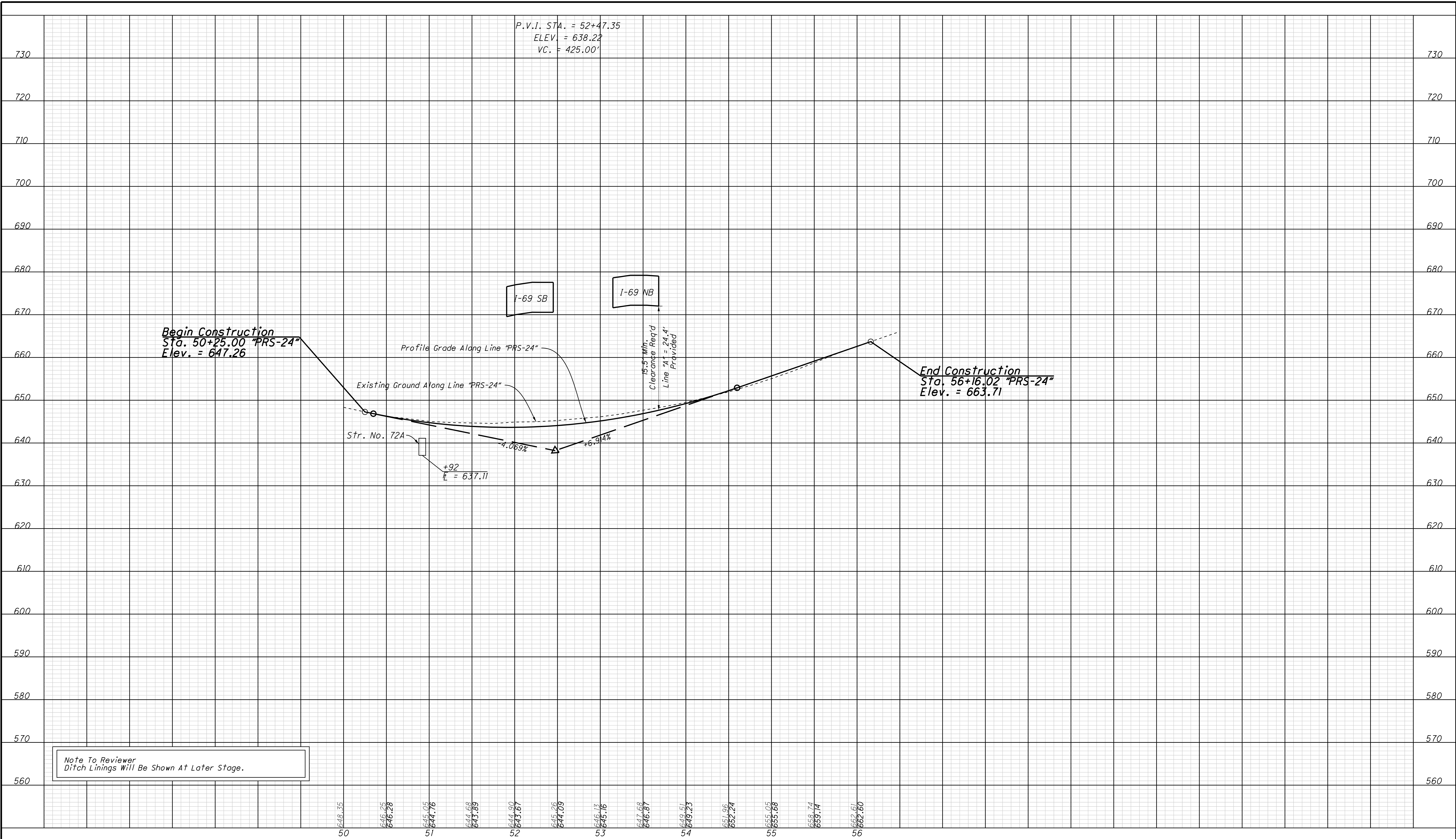
CHECKED: JAC

INDIANA
DEPARTMENT OF TRANSPORTATION

PLAN SHEET
STA. 50+25.00 TO STA. 56+16.02 "PRS-24"

HORIZONTAL SCALE 1" = 50'	BRIDGE FILE
VERTICAL SCALE N/A	DESIGNATION 1006075
SURVEY BOOK ELECTRONIC / AERIAL	PAGE PP-16
CONTRACT 1P-33742	SHEETS 51 of 53
	PROJECT 1006075

DATE: 11/16/2011
TIME: 9:28:52 AM
LOCATION: N:\Projects\25627500\Y Drawings\4 Trans\YTransp\Cadd\Road\SB\Y Plan and Profiles\Sheets\25627500\PS19-SB.dgn

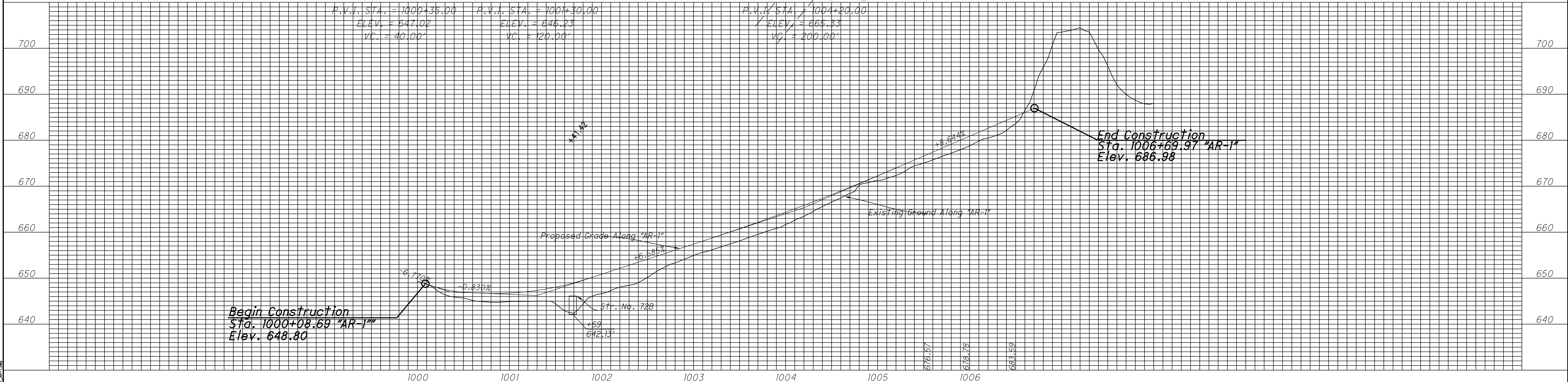
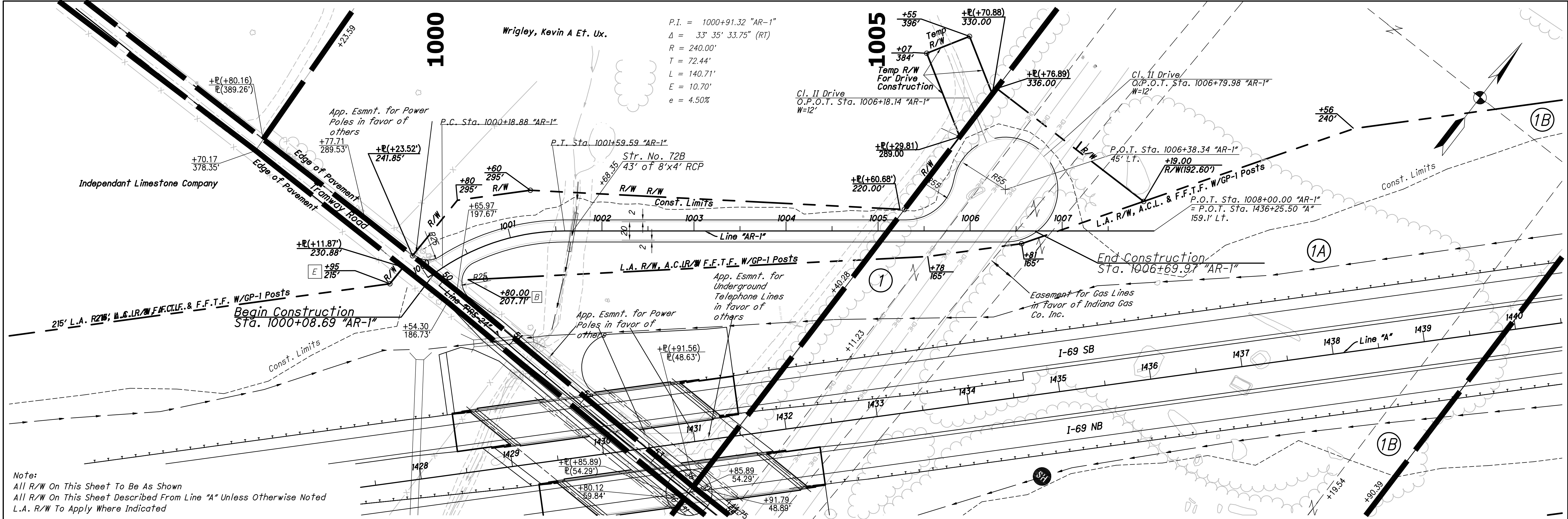


Note To Reviewer
Ditch Linings Will Be Shown At Later Stage.

RECOMMENDED FOR APPROVAL	
DESIGN ENGINEER _____ DATE _____	
DESIGNED: _____ DCK	DRAWN: _____ ETD
CHECKED: _____ AJD	CHECKED: _____ JAC

INDIANA DEPARTMENT OF TRANSPORTATION	
PROFILE SHEET STA. 50+25.00 TO STA. 56+16.02 "PRS-24"	

HORIZONTAL SCALE 1"=50'		BRIDGE FILE	
VERTICAL SCALE 1"=10'		DESIGNATION 0500450	
SURVEY BOOK ELECTRONIC / AERIAL		PAGE PS19	
CONTRACT IR-33741		SHEETS 60 of 66	
		PROJECT 0500450	



Legend

- Building Removal
- Environmental Restriction
- Tree Clearing Restriction
- Remove Existing Roadway
- Beginning L.A. R/W
- Ending L.A. R/W

RECOMMENDED FOR APPROVAL

DESIGN ENGINEER: _____ DATE: _____

DESIGNED: DCK DRAWN: ETD

CHECKED: AJD CHECKED: JAC

INDIANA DEPARTMENT OF TRANSPORTATION

PLAN SHEET LINE "AR-1"

HORIZONTAL SCALE	BRIDGE FILE
1" = 50'	
VERTICAL SCALE	DESIGNATION
N/A	1006075
SURVEY BOOK	PAGE
ELECTRONIC / AERIAL	53 of 53
CONTRACT	PROJECT
IP-33742	1006075